



CITY OF WILDOMAR

PLANNING COMMISSION

Commission Members

Chairman Robert Devine · Vice-Chairman Harv Dykstra

Gary Andre · Scott Nowak · Miguel Casillas

REGULAR MEETING

WEDNESDAY, SEPTEMBER 16, 2009 AT 7:00 P.M.

Council Chambers, Wildomar City Hall, 23873 Clinton Keith Road, Wildomar, CA 92595

PUBLIC COMMENTS: Prior to the business portion of the agenda, the Planning Commission will receive public comments regarding any agenda items or matters within the jurisdiction of the governing body. This is the only opportunity for public input except for scheduled public hearing items. The Chairperson will separately call for testimony at the time of each public hearing. If you wish to speak, please complete a "Public Speaker/Comment Card" available at the door. The completed form is to be submitted to the Chairperson prior to an individual being heard. Lengthy testimony should be presented to the Planning Commission in writing (8 copies) and only pertinent points presented orally. The time limit established for public comments is three minutes per speaker or less if a large number of requests are received on a particular item.

AGENDA

1.0 CALL TO ORDER

- 1.1 Roll Call
- 1.2 Pledge of Allegiance

2.0 PUBLIC COMMENT: *Members of the audience may comment on matters that are not included on the agenda. Each person will be allowed three (3) minutes or less if a large number of requests are received on a particular item. No action may be taken on a matter raised under "public comment" until the matter has been specifically included on an agenda as an action item.*

3.0 APPROVAL OF MINUTES:

- 3.1 Approval of the September 2, 2009 Regular Meeting Minutes.

4.0 CONTINUED PUBLIC HEARING ITEMS: The Planning Commission will review the proposed request, receive public input and consider action for the following items:

None.

5.0 PUBLIC HEARING ITEMS: The Planning Commission will review the proposed request, receive public input and consider action for the following items:

5.1 REVISION 3 TO PUBLIC USE PERMIT NO. 778 (08-0163): The project proposes the construction of a 754 space parking lot expansion, the construction of athletic fields and reconstruction of a retention basin on a 63.01 acre site at 34570 Monte Vista Drive, APNs: 367-210-008, 367-210-018, 367-210-034, 367-210-035, 367-210-041, 367-210-043, 367-140-008, & 367-140-010.

Environmental Determinations: In accordance with the California Environmental Quality Act (CEQA) a Mitigated Negative declaration has been recommended for adoption.

6.0 GENERAL BUSINESS ITEMS:

None.

7.0 ADMINISTRATIVE HEARINGS REPORT:

None.

8.0 PLANNING DIRECTOR'S REPORT: This item is reserved for the Planning Director to comment or report on items not on the agenda. No action will be taken.

9.0 PLANNING COMMISSION COMMENTS: This portion of the agenda is reserved for Planning Commission business, for the Planning Commission to make comments on items not on the agenda, and/or for the Planning Commission to request information from staff.

10.0 ADJOURNMENT

The next scheduled Regular Meeting of the City of Wildomar Planning Commission is October 7, 2009 at 7:00 P.M.

RIGHT TO APPEAL: Any decision of the Planning Commission may be appealed to the City Council within ten (10) calendar days after the date of Planning Commission's action.

REPORTS: All agenda items and reports are available for review at Wildomar City Hall, 23873 Clinton Keith Road, Suite 201, Wildomar, California 92595. Any writings or documents provided to a majority of the Planning Commission regarding any item on this agenda (other than writings legally exempt from public disclosure) will be made available for public inspection at City Hall during regular business hours. If you wish to be added to the regular mailing list to receive a copy of the agenda, a request must be made through the Planning Department in writing or by e-mail.

PUBLIC COMMENTS: Prior to the business portion of the agenda, the Planning Commission will receive public comments regarding any agenda items or matters within the jurisdiction of the governing body. This is the only opportunity for public input except for scheduled public hearing items. The Chairperson will separately call for testimony at the time of each public hearing. If you wish to speak, please complete a "Public Speaker/Comment Card" available at the door. The completed form is to be submitted to the Chairperson prior to an individual being heard. Lengthy testimony should be presented to the Planning Commission in writing (8 copies) and only pertinent points presented orally. The time limit established for public comments is three minutes per speaker.

ADDITIONS/DELETIONS: Items of business may be added to the agenda upon a motion adopted by a minimum 2/3 vote finding that there is a need to take immediate action and that the need for action came to the attention of the City subsequent to the agenda being posted. Items may be deleted from the agenda upon request of staff or upon action of the Planning Commission.

ADA COMPLIANCE: If requested, the agenda and backup materials will be made available in appropriate alternative formats to persons with a disability, as required by Section 202 of the Americans With Disabilities Act of 1990 (42 U.S.C. Sec. 12132), and the federal rules and regulations adopted in implementation thereof. Any person who requires a disability-related modification or accommodation, including auxiliary aids or services, in order to participate in the public meeting may request such modification, accommodation, aid or service by contacting the Planning Department either in person or by telephone at (951) 667-7751, no later than 10:00 A.M. on the day preceding the scheduled meeting.

POSTING STATEMENT: On September 11, 2009, a true and correct copy of this agenda was posted at the three designated posting places: Wildomar City Hall, 23873 Clinton Keith Road; U. S. Post Office, 21392 Palomar Street; and the Mission Trail Library, 34303 Mission Trail Blvd.

**REGULAR MEETING
CITY OF WILDOMAR
PLANNING COMMISSION
September 2, 2009**

1.0 CALL TO ORDER

The regular meeting of the Wildomar Planning Commission was called to order by Planning Commission Chairman Devine at 7:00 P.M. at Wildomar City Hall, Council Chambers.

1.1 ROLL CALL OF PLANNING COMMISSION

Present: Robert Devine, Chairman
Gary Andre, Commissioner
Harv Dykstra, Vice-Chairman
Scott Nowak, Commissioner
Miguel Casillas, Commissioner

Absent: None.

Staff Present: David Hogan, Planning Director
Thomas Jex, Assistant City Attorney
Jon Crawford, Supervising Engineer
Alia Kanani, Planner
Sean del Solar, Planner

1.2 PLEDGE OF ALLEGIANCE:

Commissioner Nowak led the flag salute.

2.0 COMMUNICATIONS FROM THE PUBLIC:

None.

3.0 APPROVAL OF MINUTES:

Commissioner Nowak moved to approve the Minutes of August 19, 2009. The motion was seconded by Commissioner Andre. Motion carried, the following vote resulted:

AYES: Devine, Dykstra, Nowak, Casillas, and Andre.

NOES:

ABSENT:

ABSTAIN:

4.0 CONTINUED PUBLIC HEARING ITEMS:

None.

5.0 PUBLIC HEARING ITEMS:

5.1 CHANGE OF ZONE AND PLOT PLAN NO. 08-0133.

Applicant: LNT Development LLC.
Location: 35951 Salida del Sol (APN: 362-250-014).
Proposals: The project proposes the construction of a 6,000 square foot Veterinary Hospital, a 4,500 square foot rough graded pad and a change of zone from Rural Residential (R-R) to Industrial Park (I-P).

Environmental
Action: In accordance with the California Environmental Quality Act (CEQA), a Mitigated Negative Declaration has been recommended for adoption.

Planner Kanani made the staff report.

Vice-Chairman Dykstra asked why the County Board of Supervisors originally denied the project.

Planner Kanani replied that the County originally denied the project because it was not consistent with the General Plan at that time.

Vice-Chairman Dykstra asked if the project was now consistent with General Plan.

Planner Kanani responded in the affirmative.

Vice-Chairman Dykstra noted the site of a future Community College across the street from the subject site and asked about street improvements for pedestrians at the subject site.

Supervising Engineer Crawford responded that the project was required to develop sidewalks or pay an in-lieu fee so that sidewalks could be installed at a later date when other street improvements were to be made to Salida del Sol.

Commissioner Andre stated that he agreed with Staff's recommendation that the project should connect to the sanitary sewer system. He went on to ask about the materials composing the driveway.

Planner Kanani indicated that the front driveway was paved and the driveway in the rear was composed of Decomposed Granite.

Chairman Devine asked about the location of a parcel zoned industrial near the subject site.

Planner Kanani referenced a zoning map exhibit for the project.

Chairman Devine then inquired if the development of the parcel south of the subject site, adjacent to Clinton Keith was imminent.

Planner Kanani responded that the City was not processing any development applications for that parcel.

Chairman Devine asked about the landscaping proposed for the project.

Planner Kanani responded that the project would include ornamental landscaping near the buildings and driveways and native landscapes on the other large undeveloped portions of the property to reduce weeds, nuisance vegetation and enhance the visual appearance of the property.

Chairman Devine opened the Public Hearing.

Applicant Jack Monroe addressed the Commission. He discussed the project and provided details about the use of native landscaping, the placement of the septic system, driveway materials to be used, and right-of-way widths for Salida del Sol. Mr. Monroe went on to explain the expense of bringing water and sanitary sewer infrastructure north to the project site from Clinton Keith Road and characterized these expenses as preventing the applicant from connecting to the water and sewer system. Mr. Monroe concluded by accepting the majority of the Conditions of Approval but objected those requiring the project to connect to water, citing costs.

Commissioner Nowak asked the applicant if preventative measures would be implemented to preclude the erosion of the Decomposed Granite (DG) driveways into the street.

Applicant Monroe responded that the Water Quality Management Plan outlined the use of riprap transitions between DG driveways and paved surfaces to contain any DG particles.

Commissioner Nowak went on to state that he would prefer the driveway be paved all the way north to the employee parking facilities.

Commissioner Andre again stated that he would prefer to have the project connected to both sewer and water. He then referred the applicant to Rob Hicks at the Santa Rosa Plato for more information about native species of plants.

Vice-Chairman Dykstra inquired about the voltage of the power lines which cross the site. The applicant responded that they were investigating that matter with Southern California Edison.

Vice-Chairman Dykstra stated that the proposed street improvements were acceptable and then inquired about the location of the well on the site. The applicant responded that the location of the well was still yet to be determined. Vice-Chairman Dykstra then stated that he was uncomfortable approving a project without the inclusion of a well on the plot plan.

Vice-Chairman Dykstra asked if a fire hydrant would be located within 500 feet of the proposed site. The applicant responded that he was meeting with the Fire Department next week to resolve this issue.

Commissioner Casillas asked about signage for the project.

Planning Director Hogan responded that the applicant has indicated that only building signage would be used.

Chairman Devine asked if anyone in the audience would like to provide testimony for the project before the Public Hearing was closed.

Chairman Devine closed the Public Hearing.

Chairman Devine disclosed that the project applicant was his family Veterinarian.

Commissioner Andre indicated that he was uncomfortable with the use of a DG driveway, the location of the septic tank and the timing of the construction of the sidewalk. He concluded by complimenting the applicant on the appearance of the proposed building.

Chairman Devine asked about the use of riprap to prevent erosion of the DG materials.

Applicant Monroe responded that the construction plans would address these issues in greater detail.

Supervising Engineer Crawford discussed water quality issues associated with paved and DG surfaces.

Commissioner Nowak commented that he would prefer that the project pave the rear employee parking lot so that the conditions were better for employees. He then suggested to the applicant that the use of fire resistant materials may mitigate some of the fire hazards in the area.

Chairman Devine asked the Commission to comment on the placement of the well on the site and the connection to sewer and water services.

Vice-Chairman Dykstra indicated that he felt favorably about the use of pervious surfaces but acknowledge that Commissioner Andre's concerns about the erosion of DG materials at the project site were valid. He then suggested that the applicant be required to connect to water and sewer services when they become available on Salida del Sol. Vice-Chairman Dykstra concluded by confirming that sidewalks will need to be provided at an appropriate time and requested that the power lines at the site be undergrounded.

Commissioner Andre asked about the location of the well and asked the applicant about the cost of the well. The applicant responded that in total, the proposed system would cost about \$45,000 and a connection to water and sewer would cost in excess of \$200,000.

Commissioner Andre then discussed flooding issues near the location of the leach field site and suggested that the placement of the system should be reconsidered.

Commissioner Nowak asked if there was any potential of alleviating the cost of installing the sewer and water infrastructure.

Supervising Engineer Crawford stated that he would need to direct that question to the Elsinore Valley Municipal Water District (EVMWD).

Commissioner Nowak stated that he would prefer that the project connects to EVMWD sewer and water, however he recognized the significant cost. He concluded by stating he felt favorably about Vice-Chairman Dykstra's suggestion that the applicant be conditioned to connect to sewer and water services when available.

Chairman Devine asked whom would bear the costs of installing water and sewer infrastructure along Salida del Sol.

Supervising Engineer Crawford responded that it was based on a project by project basis, incrementally advancing.

Commissioner Andre asked when the Department of Environmental Health would check the septic system and well. Supervising Engineer Crawford responded that the Department of Environmental Health would be involved if and when the applicant pulls permits for the well and septic system.

Vice-Chairman Dykstra expressed a reluctance to require that the applicant connect to the EVMWD water and sewer system citing the significant cost. He then asked if the applicant could be required to connect at a future time when the infrastructure was installed on Salida del Sol. Supervising Engineer Crawford responded that the City could condition for a future connection, but pointed out that it would be difficult to implement.

Commissioner Nowak added that when the connection would occur in the future, the construction and installation of the line would be very disruptive to the business.

Vice-Chairman Dykstra asked if the applicant could bond for the water and sewer improvements with the City. Supervising Engineer Crawford responded that it would be difficult because the City did not control the water and sewer systems and that he was unaware of EVMWD engaging in such an arrangement.

Chairman Devine indicated that he was in favor of Vice-Chairman Dykstra's suggestion that a future connection to the water and sewer system be required.

Applicant Monroe discussed the design of the project and how after-the-fact construction of water and sewer connections could be accomplished.

Chairman Devine asked what the approval process would be for the second building proposed at the site.

Planning Director Hogan responded that if anything was built, the project would be considered a substantial conformance and would not need to come before the Commission. Mr. Hogan went on to suggest to the Commission that since their actions on this item would be a recommendation to the City Council, the Commission could require the connection to sewer and water and the City Council could consider the

condition of approval with additional information at the City Council hearing on September 9.

Commissioner Nowak asked which resolution contained the condition to connect to the water system. Planning Director Hogan responded that it was contained in the resolution for the plot plan.

Chairman Devine recognized the uncertainty about the connection to water and sewer but suggested that the Commission should vote and provide a recommendation to the City Council. Planning Director Hogan advised the Commission, that a condition for the project addressing the connection to the water and sewer systems may be altered at the City Council hearing based on additional information from the Fire Department.

Vice-Chairman Dykstra complimented the quality of the project.

Chairman Devine asked for clarification on the logistics of the vote and Planning Director Hogan responded.

MOTION: Commissioner Nowak motioned to approve resolution PC09-016 recommending the City Council adopt Mitigated Negative Declaration number 08-0133. The motion was seconded by Vice-Chairman Dykstra. Motion carried, the following vote resulted:

AYES: Devine, Andre, Dykstra, Nowak and Casillas.
NOES:
ABSENT:
ABSTAIN:

MOTION: Vice-Chairman Dykstra motioned to approve resolution PC09-017 recommending the City Council approve Change of Zone 08-0133. The motion was seconded by Commissioner Andre. Motion carried, the following vote resulted:

AYES: Devine, Andre, Dykstra, Nowak and Casillas.
NOES:
ABSENT:
ABSTAIN:

Commissioner Nowak asked for clarification on the adoption of the resolution and Planning Director Hogan discussed the options available to the Commission.

Chairman Devine noted that the Fire Department had not yet provided comprehensive comments yet and then commented that while although a well and septic system might

be acceptable now, it would not be ideal later, especially when the second structure is constructed at the site.

MOTION: Commissioner Nowak motioned to approve resolution PC09-018 recommending the City Council approve plot plan 08-0133 as amended at the hearing (to allow septic and well systems until sewer and water connections were available). The motion was seconded by Vice-Chairman Dykstra. Motion carried, the following vote resulted:

AYES: Devine, Andre, Dykstra, Nowak and Casillas.

NOES:

ABSENT:

ABSTAIN:

6.0 GENERAL BUSINESS ITEMS:

6.1 PRESENTATION ON THE WILDOMAR RECYCLED WATER PROJECT BY EVMWD.

EVMWD Engineering Director Paul Carver made the presentation on the project.

Commissioner Andre asked when the reclaimed water plant was constructed. EVMWD engineer Carver responded it was constructed 10 years ago.

Vice-Chairman Dykstra asked about other sites in the City that could connect to the recycled water line. EVMWD engineer Carver responded that the sites were limited to ones located in close proximity to the pipeline.

Commissioner Nowak asked for confirmation that all the sites were west of the Freeway. EVMWD engineer Carver responded in the affirmative.

Supervising Engineer Crawford informed the Commission that he and the Planning Director had copies of the Plans available at City Hall.

7.0 ADMINISTRATIVE HEARINGS REPORT:

None.

8.0 PLANNING DIRECTOR'S REPORT:

Director Hogan noted the move to the new Council Chambers and commended the Commission for the deliberation and decision made pertaining to project 08-0133.

9.0 PLANNING COMMISSION COMMENTS:

Chairman Devine agreed with Director Hogan and thanked the Commission.

10.0 ADJOURNMENT:

The September 2, 2009 regular meeting of the Wildomar Planning Commission adjourned at 8:50 P.M.

Respectfully submitted:

David Hogan
Commission Secretary

CITY OF WILDOMAR – PLANNING COMMISSION
Agenda Item # 5.1
PUBLIC HEARING
Meeting Date: September 16, 2009

TO: Members of the Planning Commission

FROM: David Hogan, Planning Director

SUBJECT: Project Number 08-0163 – Cornerstone Community Church Parking Lot Expansion

Assessors Parcel Numbers: 367-210-008, 367-210-018, 367-210-034, 367-210-035, 367-210-041, 367-210-043, 367-140-008, & 367-140-010

RECOMMENDATION:

The Planning Department recommends that the Planning Commission:

1. Adopt a resolution entitled:

“A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF WILDOMAR CERTIFYING THE MITIGATED NEGATIVE DECLARATION FOR THE THIRD REVISION TO PUBLIC USE PERMIT 778, THE CORNERSTONE COMMUNITY CHURCH PARKING LOT EXPANSION (PROJECT NO. 08-0163)”

2. Adopt a resolution entitled:

“A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF WILDOMAR APPROVING THE THIRD REVISION TO PUBLIC USE PERMIT 778, THE CORNERSTONE COMMUNITY CHURCH PARKING LOT EXPANSION (PROJECT NO. 08-0163)”

BACKGROUND:

The Public Use Permit authorizing the operation of Cornerstone Community Church was first approved on October 21, 1996. Since its approval, the facility has received approval for two facility expansions. The current church and school activities are conducted in three buildings located on the western portion of the site adjacent to Monte Vista Road. Cornerstone Community Church is located east of Interstate 15 and Monte Vista Drive between Bundy Canyon Road and Baxter Road on the western side of the prominent local ridgeline which parallels the freeway. The location of the project site is shown on Attachment C.

The proposed project, the third revision to the Public Use Permit 778 would authorize the construction of an additional 764 parking spaces, the construction of an onsite detention basin, and the construction of replacement football/soccer and baseball fields. The additional parking spaces are necessary to support to the activities and functions associated with the existing church and school facility. The project includes the improvement of Via Carnaghi to a rural Local Street standard (32 feet of pavement without curb and gutter) and half width frontage improvements to the church frontage along Monte Vista Road. The construction of these facilities would also require the export of approximately 700,000 cubic yards of soil.

The entire site for Cornerstone Community Church incorporates eight parcels ranging in size from 0.84 acres to 28.45 acres. The total area for the eight parcels is 83.01 acres. The new parking lot and the replacement athletic fields will occur on the four eastern most parcels while the proposed detention basin will be on two parcels located south of the existing complex of buildings. A single family residence located on a church-owned parcel adjacent to the existing eastern parking lot and Via Carnaghi Lane is not being altered by the project. The proposed replacement athletic fields will be illuminated for nighttime use. The proposed lighting consists of six 70-foot tall lighting structures around the baseball diamond (four around the infield and two in the outfield) and four 70-foot tall lighting structures (two on each side) around the proposed football/soccer field.

The existing church/school facility is designated as Business Park on the General Plan. The expansion areas are primary designated as Medium Density (2-5 du/ac) and Estate Density Residential (0.5 du/ac). The primary ridgeline east of the church is located within the Estate Density Residential area. The proposed haul route from the grading area to Monte Vista Road is located over an adjacent parcel north of the church complex (APN 367-140-010). The designations on this property are a combination of Business Park and Medium Density Residential.

The information on the land uses, General Plan Land Use Designations, and Zoning Districts for the project site and surrounding area are described below. A vicinity map with the surrounding General Plan Land Use Designations is in Attachment D. The church property boundary is surrounded with a dark line on the Attachment.

EXISTING LAND USE, GENERAL PLAN, AND ZONING INFORMATION			
Location	Current Land Use	General Plan Land Use Designation	Zoning
Site	Church/School complex & vacant	Business Park, Medium Density Residential, & Estate Density Residential	Rural Residential (R-R)
North	Vacant & residential	Estate Density Residential, Medium Density Residential, & Business Park	Single Family Residential (R-1), Open Space Combining, Residential (R-5), & Rural Residential (R-R)

EXISTING LAND USE, GENERAL PLAN, AND ZONING INFORMATION			
Location	Current Land Use	General Plan Land Use Designation	Zoning
South	Vacant, commercial, & residential	Commercial Retail & Business Park	Rural Residential (R-R) & Scenic Highway Commercial (C-P-S)
East	Vacant & residential	Estate Density Residential & Medium Density Residential	Rural Residential (R-R)
West	Commercial, residential, and Interstate 15	Business Park & Medium Density Residential	Rural Residential (R-R)

Because the Cornerstone Community Church includes both religious and educational functions, the facility has operations occurring seven days a week use during the school year. As demonstrated below, most of the Monday through Friday activities consist of school functions during the day and social/recreational programs in the evening. Weekend activities are primarily worship, education and fellowship related. A typical weekly schedule of activities, with common attendance figures, is described below (with slightly higher than typical attendance on Tuesday and Friday evenings). This typical weekly activity information is provided to the Planning Commission solely for informational purposes, it not part of proposed project.

DAYS	ACTIVITIES	HOURS	ATTENDANCE
Monday - Friday	K-12 School	7:30 am to 3:00 pm	325 - 350
	After school program activities (both indoor and outdoor)	3:00 pm to 6:00 pm	50 - 75
	Bible study, social and youth programs	6:00 pm to 9:00 pm	50 - 200
	Sports activities (both indoor and outdoor)	7:00 pm to 10:00 pm	40 - 50
Saturday	Sports activities (both indoor and outdoor)	7:00 am to 5:00 pm	100 - 300
	Worship planning, rehearsal, and service set-up	7:00 am to 4:30 pm	20 - 50
	Church Service (worship and education)	5:00 pm to 8:00 pm	700 - 800
Sunday	2 Church Services (worship and education)	8:00 am to 12:00 pm	2,000 - 2,300 [Total]
	Leadership, worship and education-related meetings	12:00 pm to 3:00 pm	20 - 30
	Church Service (worship and education)	6:30 pm to 8:30 pm	100 - 300

In addition to the typical weekly and ongoing activities described above, there are occasional special entertainment events and school functions throughout the year.

According to Pastor Rosen, there are typically 1 to 2 of these activities each month. The majority of these special activities occur indoors.

The primary outdoor functions involve the continuing use of the athletic fields for high school and youth/adult sports teams. At the present time, Cornerstone Community Church is using temporary lighting for athletic on the existing field areas between Via Carnaghi and Monte Vista Road when the fields are in use during the evening. The proposed field lighting would allow the use of the new athletic fields during the evening. Because of the shorter days there are expected to be more lighted evening sporting events during the winter/shorter day months and fewer lighted evening sporting events during the summer months. According to the Church, they anticipate that there will be 1 to 2 illuminated evening events per week in the summer because the longer daylight hours would require less artificial lighting, and 2 to 4 illuminated evening events during the winter. The anticipated evening use of athletic fields includes 5 to 6 Church high school football home games per year. The use and illumination of the athletic fields are appropriate issues for consideration with the Third Amendment to Public Use Permit 778.

This project was considered and continued by the Riverside County Planning Commission on April 30, 2008 and June 11, 2008. At the June 11th meeting, the project was continued to be considered by the City of Wildomar. The key issues of concern during these meetings were the amount of grading, the potential for traffic congestion problems, and concerns about potential lighting on the athletic fields (even though the night time field lighting was not part of the project in early 2008). Because of the length of time this project has been in process, the Church needs have changed and athletic field lighting is now part of the project.

DISCUSSION:

The proposed third revision to Public Use Permit 778 centers on the construction of additional needed off-street parking to support the current operations of the Cornerstone Community Church. As a result of changing needs over time, the project now includes the following items.

- Construction of an expanded parking lot to meet current needs (including 754 parking spaces, security lighting and landscaping).
- Construction of a water quality detention basin (as shown on earlier plans) where the athletic field/overflow parking area is currently located.
- Construction of replacement athletic fields for football/soccer and baseball (including the placement of non-permanent bleachers).
- Installation of nighttime activity lighting around the new athletic fields.
- The use of the athletic fields by other members of the community and non-church members.
- The grading of the project site to construct the parking area and athletic fields.

- The export of approximately 700,000 cubic yards of materials (required by the construction of the parking lot and athletic fields).
- Street improvements to Via Carnaghi (32 feet of pavement without curb gutter and sidewalk).
- Street improvements to Monte Vista Road along the project frontage (includes the dedication of an additional 38 feet of right-of-way with an additional 22 feet of pavement with curb, gutter, sidewalk, and a 10 foot wide community trail).
- Revegetation of the graded slope areas.

Because of the wide variety of project components, staff has attempted to summarize the potential issues associated with the Cornerstone Community Church parking lot expansion. A brief description of the potential impacts and concerns, and a staff recommendation as to whether or not the item is potential concern is provided below. A reduction of the project plans are contained in Attachment E.

PROJECT COMPONENT	POTENTIAL IMPACTS/CONCERNS	ISSUE*
Construction of an expanded parking lot	Create additional urban runoff. The additional parking will reduce any existing parking impacts to the surrounding area.	No
Construction of a detention basin	None. The detention basin will reduce impacts to the water quality from urban runoff.	No
Construction of new athletic fields	Additional noise and drainage onto off-site properties. (Also see the lighting and field usage discussions provided below.)	No
Athletic field lighting	Lighting impacts to adjacent properties and roadways. Additional noise from evening sporting events.	Yes
Non-church athletic field use	Additional noise and additional local vehicle trips over the levels projected for church-only use.	Yes
Project grading	Air quality (dust/particulate matter) and noise.	No
Soil export operation	Air quality (dust and particulate matter), damage to local roads, traffic congestion, and potential environmental impacts at the anticipated fill sites.	No
Improvements to Via Carnaghi	Scale and timing of the improvements. The street improvements will help reduce existing air quality (dust) impacts.	No
Improvements to Monte Vista Road	None.	No
Revegetation of graded slope areas	Use of drought-tolerant native species and fire protection needs.	No

* A "No" means that staff believes that any potential impacts are either minimal, can easily be addressed through the standard requirements relating to project grading and construction, and/or do not create an impact on the community.

Parking Area Expansion

The construction of the expanded parking lot is a key component of this project. It is also the existing need of Cornerstone Community Church that initiated this project in 2005. The church currently has 376 permanent parking spaces. These existing spaces are adequate for most of the facility's uses except during peak times. During periods of peak church use, the existing athletic field and a church-owned vacant lot on Via Carnaghi are also used to provide parking. The peak use period for the church is on Sunday mornings, especially during the transition period between the two morning services. It is this overlap period that additional parking is required on-site. The parking area expansion is proposed to occur east and southeast of the existing upper parking lot (located east of the church/school building complex) and would provide an additional 754 parking spaces. If the proposed expansion is approved by the Planning Commission, the church/school facility will have 1,113 regular and 17 handicapped parking spaces. Access to the existing and proposed parking areas will be via the existing driveways onto Monte Vista Road and, additionally on Sunday mornings through Via Carnaghi. The development of the parking lot includes the associated security lighting (which will comply with the provisions of the Mt Palomar Lighting Ordinance) and the parking area landscaping. The landscaping is shown on the conceptual landscape plan contained in Attachment F. Staff believes that the expansion to the existing parking is appropriate and necessary.

Water Quality Detention Basin

Current State requirements to address water quality require that urban runoff (from roof tops and parking lots) be treated and detained on site. Given the current state of the subject property and the location of the proposed parking lot expansion, the existing athletic field is an ideal location for the required detention basin. In fact, a detention basin had been shown in that location on earlier plans; however, the County allowed the athletic field to be constructed here instead. The exact size and design of the basin will be determined by the City Engineer based upon the information contained in the Final Water Quality Management Plan. Staff is not concerned with the location or design of the proposed detention basin.

Replacement Athletic Fields

Since construction of the detention basin will eliminate the existing athletic fields, the project includes the construction of new football/soccer and baseball fields elsewhere on site. These fields are proposed to be located on the southern portion of the site. This area is located between the existing ridgeline, several vacant commercial properties, and several existing residences along Via Carnaghi. The project would include the installation of semi-permanent bleachers and drinking fountains. While not specified, staff believes that restroom facilities will also be required near the fields and that the exact location can be approved during project construction. Staff is not concerned with the replacement of the existing athletic fields since there is a lack of similar recreational amenities within the community (when the issues of the nighttime

illumination and non-church/school use are separated). By not replacing the existing athletic fields staff believes that the project could adversely affect the community by eliminating athletic fields that are used by the congregation, the church operated youth and adult sports leagues, and by the Church school (including the high school-level sports teams). As a result, staff feels that replacing the athletic fields is appropriate, and given the physical constraints of the site, the proposed location is probably best for these amenities.

Athletic Field Lighting

Cornerstone Community Church is requesting approval to install night time sports field lighting adjacent to the football/soccer and baseball fields. Night time illumination of the athletic fields would allow for evening use of these facilities by members of the congregation and church school sports teams. According to Pastor Rosen, the illumination of the athletic fields will allow for high school football home games. The illumination of the athletic fields, combined with the potential non-congregational/non-school field use, have the greatest potential to affect the quality of life for nearby residents since the construction impacts will be of relatively short duration. However, since these two issues are separable, staff is recommending that the Commission consider both issues separately.

The proposed lighting around the football/soccer and baseball fields has been designed to focus the light onto the fields of play and minimize the amount of off-site "leakage." To demonstrate this, the applicant has had a lighting study prepared. The light study indicates lighting levels off of the property will be generally less than one foot-candle (i.e. the illumination of a single candle at a distance of one foot). A copy of the lighting study is contained in Attachment G.

If the Planning Commission feels that evening use of the recreational amenities by members of the congregation and the church school is appropriate on an ongoing basis, then staff recommends that the Commission approve the installation of the proposed athletic field lighting. However, staff recommends that a condition of approval be added which requires that the (non-security) lights be turned off at 10:00 pm each evening. This provision is similar to the requirements in the surrounding communities which turn off athletic field lighting at 10:00 pm.

Non-church athletic field use

Staff was initially concerned that use of the recreational amenities at Cornerstone Community Church by non-church organizations could create additional evening activities which would result in additional potential impacts during the evening hours. This concern has also been expressed by some local residents. According to Pastor Rosen, the Church will not be renting their athletic fields to outside use. The primary use of the athletic fields will be Church operated youth and adult sports leagues which Cornerstone Community Church describes as a "recreational ministry." If the Commission is concerned about the future non-church use of the facilities, staff

recommends that a condition of approval be added indicating that non-church/school use of the athletic fields be prohibited unless specifically authorized by the City.

Project Grading

The grading of the project site is the primary site-modifying activity of the project. The grading is necessary to create the areas for the parking lot and athletic fields and is expected to take several years (because of the haul/export restrictions by the City to avoid air quality issues and minimize traffic congestion). The proposed grading would move approximately 700,000 cubic yards of material off of the project site to various "fill" sites in the surrounding area. The project has been conditioned to comply with all of the standard requirements which minimize off-site impacts and effects of the wind and water erosion. The project has been conditioned to comply with the detailed provisions of the required grading plan. These provisions include restrictions on the hours of the grading operations to Monday through Friday between the hours of 7:00 am and 6:00 pm. As a result, staff believes that with these requirements in place, any effects on the surrounding community will be minimal.

Soil Export Operation

In conjunction with the project grading, the proposed off-site hauling of excess soil material from the project site has the potential to have adverse impacts to the surrounding area. While most hauling operations are of fairly short duration, the anticipated restrictions on the hauling activities from the proposed project will extend the duration of this activity over a longer than usual period. The anticipated restrictions to the haul activities will focus on limiting the number of daily haul trips to prevent any exceedences of the South Coast Air Quality Management District's significance thresholds and limits to the hours of operation to limit truck trips during peak traffic congestion periods. According to the air quality analysis prepared for the project, no more than 116 twenty mile haul trips will be allowed each day. These requirements will be addressed through a condition of approval requiring that the haul permit(s) be approved prior to any soil being removed from the site.

Improvements to Via Carnaghi

To facilitate the Sunday operation of the church, Cornerstone Community Church has proposed to pave Via Carnaghi for its entire length to the intersection with Monte Vista Road. Minimal use of Via Carnaghi is expected to occur during the rest of the week as the majority of the use would occur on the weekend. The Church proposed to install 24 feet of paving for the road surface; however, the City Engineer has determined that this width is not adequate. Instead the City has conditioned that the project provide 32 feet of paved road surface would create a safer and better designed street.

Improvements to Monte Vista Road

The project is required to improve its street frontage along Monte Vista Road. These improvements include the following: an additional 38 feet of right-of-way dedication, the

construction of an additional 22 feet of payment, the construction of curb, gutter, sidewalk, and a 10-foot wide community trail with a split rail fence. These improvements will improve Monte Vista Road in front of the existing church property to their ultimate width. The project is conditioned to construct these improvements.

Revegetation of Graded Slopes

The project proposes three different approaches to the on-site landscaping based upon the location and purpose of the landscaped area. The three different landscaped areas are as follows:

- Parking lot and lower slope areas:
- Upper slope revegetation: and,
- Athletic fields turf.

The landscaping in and around the parking lot (including the lower slopes and the perimeter landscaping west and south of the athletic fields) will be a traditional mixture of shade and screening trees combined with shrubs and ground cover. The revegetation of the upper cut slopes will be centered-around native trees and shrubs to mimic similar areas with native vegetation. The purpose of this landscaping is to blend these slopes into the natural hillsides so that the manufactured slope areas appear to be natural. The last landscape area is the turf area around the athletic fields.

A conceptual landscape plan was submitted by the applicant and reviewed by the City's landscape architect. The recommendations of the landscape architect emphasized the creation of a low water use plant pallet and the use of native species where appropriate, and have been incorporated into the conditions of approval. The construction-level plans will be required to integrate these provisions into any approval plan sets.

Recommendation

The Planning Department recommends that the Planning Commission review the project, make a determination as to the appropriateness of the athletic field lighting, and approve the project subject to the conditions of approval.

FINDINGS:

1. That the proposed location, use and operation of the public use is in accord with the purposes of the zone in which the site is located, is consistent with the General Plan and complies with other relevant city regulations, policies and guidelines.

The expansion of the parking area for Cornerstone Community Church is consistent with the provisions of the Rural Residential (R-R) Zone pursuant to the provisions for public use permits. The project is consistent with the intent of the Zoning Ordinance since it meets and/or exceeds the minimum development

standards of the R-R Zone as illustrated in the Staff Report. Additionally, conditions have been added to the project to ensure that all the minimum requirements of the Municipal Code are met.

The General Plan land use designation for the site is Medium Density Residential, Business Park, and Estate Density Residential. The General Plan and the existing zoning ordinance envision that religious institutions can be appropriately located in virtually any land use designation. The design and layout of the site, the site access and circulation, as well as the required street and drainage improvements have been configured to accommodate the development of the project site. Considering all of these aspects, the project furthers the objectives and policies of the General Plan and is compatible with the general land uses as specified in the General Plan.

2. The proposed use will not be detrimental to the health, safety, or general welfare of the community.

The site has been designed to meet all of the development standards of the Rural Residential (R-R) Zone and the other provisions of the zoning ordinance. The proposed expansion of the parking area for an existing religious institution will not be detrimental to the health, safety, and general welfare. In addition, the site does not contain any natural or physical hazards which would cause the project to be detrimental to the health, safety, or general welfare.

ENVIRONMENTAL ASSESSMENT:

An Initial Study was prepared for the Project. The Initial Study identified no significant impacts to the environment with the modifications made to the proposed project, the implementation of the applicable standards conditions and requirements, and the mitigation measures contained in the document. The Initial Study and the Notice of Intent to Adopt a Negative Declaration was circulated for public review between August 27, 2009 and September 15, 2009. A copy of the Initial Study document is contained in Attachment H.

No comments were received regarding the environmental document prior to the preparation of this staff report. Any comments received prior to the Commission meeting will be provided to the Planning Commission meeting.

ALTERNATIVES:

1. Deny the Project.
2. Approve the parking lot and athletic field components only.
3. Provide direction to staff.

ATTACHMENTS:

- A. Resolution approving the Mitigated Negative Declaration
- B. Resolution approving Plot Plan 08-0163
Exhibit A – Conditions of Approval
- C. Vicinity Map and Aerial Photograph
- D. General Plan Land Use Designations
- E. Site Development Plan (3)
- F. Conceptual Landscape Plan (2)
- G. Athletic Field Lighting Study
- H. Initial Study

ATTACHMENT A

RESOLUTION NO. PC09-_____

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF WILDOMAR CERTIFYING THE MITIGATED NEGATIVE DECLARATION FOR THE THIRD REVISION TO PUBLIC USE PERMIT 778, THE CORNERSTONE COMMUNITY CHURCH PARKING LOT EXPANSION (PROJECT NO. 08-0163)

WHEREAS, an application to allow the modification of an existing church and school complex to meet current needs on property located adjacent to the existing church/school complex was filed by:

Applicant/Owner: Cornerstone Community Church

Authorized Agent: Larry Markham,

Project Location: 34570 Monte Vista Road

APN Number: 367-210-008, 367-210-018, 367-210-034, 367-210-035, 367-210-041, 367-210-043, 367-140-008, and 367-140-010

Project Area: 83.01 gross acres

WHEREAS, the proposed parking lot expansion for Cornerstone Community Church is considered a “project” as defined by the California Environmental Quality Act, Public Resources Code § 21000 et seq. (“CEQA”);

WHEREAS, to assess the potential impacts associated with the project, an Initial Study was prepared to determine the impact of the project on the environment were expected to occur;

WHEREAS, on August 22, 2009 using a method permitted under CEQA Guidelines Section 15072(b), the City provided notice of its intent to adopt the proposed Mitigated Negative Declaration to the public, responsible agencies, trustee agencies, and the Riverside County Clerk;

WHEREAS, the City made the proposed Mitigated Negative Declaration available for public review from August 27, 2009 to September 15, 2009, and no comments were received on the Initial Study; and,

WHEREAS, the Wildomar Planning Commission conducted a duly noticed public hearing on September 16, 2009 at which it received public testimony concerning the project and the proposed Mitigated Negative Declaration.

NOW THEREFORE, the Planning Commission of the City of Wildomar does hereby resolve, determine and order as follows:

SECTION 1. FINDINGS.

The Planning Commission, in light of the whole record before it including but not limited to the City's local CEQA Guidelines and Thresholds of Significance, the proposed Mitigated Negative Declaration and documents incorporated therein by reference, any written comments received and responses provided, the proposed Mitigation Monitoring Program and other substantial evidence (within the meaning of Public Resources Code § 21080(e) and § 21082.2) within the record and/or provided at the public hearing, hereby finds and determines as follows:

A. Review Period: That the City has provided the public review period for the Mitigated Negative Declaration for the duration required under CEQA Guidelines Sections 15073 and 15105.

B. Compliance with Law: That the Mitigated Negative Declaration and Mitigation Monitoring Program were prepared, processed, and noticed in accordance with the California Environmental Quality Act (Public Resources Code Section 21000 et seq.), the CEQA Guidelines (14 California Code of Regulations Section 15000 et seq.) and the local CEQA Guidelines and Thresholds of Significance adopted by the City of Wildomar.

C. Independent Judgment: That the Mitigated Negative Declaration reflects the independent judgment and analysis of the City.

D. Mitigation Monitoring Program: That the Mitigation Monitoring Program is designed to ensure compliance during project implementation in that changes to the project and/or mitigation measures have been incorporated into the project and are fully enforceable through permit conditions, agreements or other measures as required by Public Resources Code Section 21081.6.

E. No Significant Effect: That revisions made to the project plans agreed to by the applicant and mitigation measures imposed as conditions of approval on the project, avoid or mitigate any potential significant effects on the environment identified in the Initial Study to a point below the threshold of significance. Furthermore, after taking into consideration the revisions to the project and the mitigation measures imposed, the Planning Commission finds that there is no substantial evidence, in light of the whole record, from which it could be fairly argued that the project may have a significant effect on the environment. Therefore, the Planning Commission concludes that the project will not have a significant effect on the environment.

SECTION 2. MULTIPLE SPECIES HABITAT CONSERVATION PLAN (MSHCP).

The project is found to be consistent with the MSHCP. The project is located outside of any MSHCP criteria area and mitigation is provided through payment of the MSHCP Mitigation Fee.

SECTION 3. PLANNING COMMISSION ACTIONS.

Based on the foregoing findings, and on substantial evidence in the whole of the record, the Planning Commission hereby takes the following actions:

A. Adopt the Mitigated Negative Declaration: The Mitigated Negative Declaration for project 08-0163 – Cornerstone Community Church Parking Lot Expansion consisting of 754 parking spaces, replacement athletic fields, and other private and public improvements is hereby approved and adopted.

B. Adopt Mitigation Monitoring Program: The Mitigation Monitoring Program for the Mitigated Negative Declaration is hereby approved and adopted.

C. Notice of Determination: In compliance with Public Resources Code § 21152 and CEQA Guidelines § 15075, the Planning Director shall prepare a Notice of Determination concerning the approval and adoption of the Mitigated Negative Declaration, and within five (5) working days of project approval, file the Notice with the Riverside County Clerk for posting.

D. Location: The Mitigated Negative Declaration, Mitigation Monitoring Program, and all documents incorporated therein or forming the record of decision, therefore, shall be filed with the Wildomar Planning Department at the Wildomar City Hall, 23873 Clinton Keith Rd., Suite 201, Wildomar, California 92595, and shall be made available for public review upon request.

PASSED, APPROVED AND ADOPTED this 16th day of September 2009.

Robert Devine
Chairman

APPROVED AS TO FORM:

ATTEST:

Thomas Jex
Assistant City Attorney

David Hogan
Planning Commission Secretary

ATTACHMENT B

RESOLUTION NO. PC09-_____

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF WILDOMAR APPROVING THE THIRD REVISION TO PUBLIC USE PERMIT 778, THE CORNERSTONE COMMUNITY CHURCH PARKING LOT EXPANSION (PROJECT NO. 08-0163)

WHEREAS, an application to allow the modification of an existing church and school complex to meet current needs on property located adjacent to the existing church/school complex was filed by:

Applicant/Owner: Cornerstone Community Church

Authorized Agent: Larry Markham

Project Location: 34570 Monte Vista Road

APN Number: 367-210-008, 367-210-018, 367-210-034, 367-210-035, 367-210-041, 367-210-043, 367-140-008, and 367-140-010

Project Area: 83.01 gross acres

WHEREAS, the Planning Commission has the authority per Chapter 17.200 of the Wildomar Municipal Code to take action on Public Use Permit No. 778 (08-0163); and

WHEREAS, on August 22, 2009, the City gave public notice by mailing to adjacent property owners and by placing an advertisement in a newspaper local circulation of the holding of a public hearing at which the project would be considered; and

WHEREAS, on September 16, 2009 the Planning Commission held the noticed public hearing at which interested persons had an opportunity to testify in support of, or opposition to, the Public Use Permit; and

WHEREAS, at this public hearing on September 16, 2009 the Planning Commission considered, heard public comments on, and adopted a Mitigated Negative Declaration and Mitigation Monitoring Program for the project by Resolution No. PC09-____; and

NOW THEREFORE, the Planning Commission of the City of Wildomar does hereby resolve, determine and order as follows:

SECTION 1. ENVIRONMENTAL FINDINGS.

The Planning Commission, in light of the whole record before it, including but not limited to, the City's Local CEQA Guidelines and Thresholds of Significance, the

recommendation of the Planning Director as provided in the Staff Report dated September 16, 2009, and documents incorporated therein by reference, and any other evidence (within the meaning of Public Resources Code § 21080(e) and § 21082.2) within the record or provided at the public hearing of this matter, hereby finds and determines as follows:

A. CEQA: The approval of this Public Use Permit is in compliance with requirements of the California Environmental Quality Act ("CEQA"), in that on September 16, 2009, at a duly noticed public hearing, the Planning Commission approved and adopted a Mitigated Negative Declaration and Mitigation Monitoring Program reflecting its independent judgment and analysis and documenting that there was not substantial evidence, in light of the whole record, from which it could be fairly argued that the project may have a significant effect on the environment. The documents comprising the City's environmental review for the project are on file and available for public review at Wildomar City Hall, 23873 Clinton Keith Rd., Suite 201, Wildomar, CA 92595.

B. Multiple Species Habitat Conservation Plan (MSHCP). The project is found to be consistent with the MSHCP. The project is located outside of any MSHCP criteria cell area and mitigation is provided through payment of the MSHCP Mitigation Fee.

SECTION 2. REQUIRED FINDINGS.

Pursuant to Wildomar Municipal Code, and in light of the record before it including the staff report dated September 16, 2009 and all evidence and testimony heard at the public hearing on the Third Revision to Public Use Permit 778, the Planning Commission hereby finds as follows.

1. That the proposed location, use and operation of the public use is in accord with the purposes of the zone in which the site is located, is consistent with the General Plan and complies with other relevant city regulations, policies and guidelines.

The expansion of the parking area for Cornerstone Community Church is consistent with the provisions of the Rural Residential (R-R) Zone pursuant to the provisions for public use permits. The project is consistent with the intent of the Zoning Ordinance since it meets and/or exceeds the minimum development standards of the R-R Zone as illustrated in the Staff Report. Additionally, conditions have been added to the project to ensure that all the minimum requirements of the Municipal Code are met.

The General Plan land use designation for the site is Medium Density Residential, Business Park, and Estate Density Residential. The General Plan and the existing zoning ordinance envision that religious institutions can be appropriately located in virtually any land use designation. The design and layout of the site, the site access and circulation, as well as the required street and drainage improvements have been configured to accommodate the development of the project site. Considering all of these aspects, the project furthers the objectives and policies of the General Plan and is compatible with the general land uses as specified in the General Plan.

2. The proposed use will not be detrimental to the health, safety, or general welfare of the community.

The site has been designed to meet all of the development standards of the Rural Residential (R-R) Zone and the other provisions of the Zoning Ordinance. The proposed expansion of the parking are for an existing religious institution will not be detrimental to the health, safety, and general welfare. In addition, the site does not contain any natural or physical hazards which would cause the project to be detrimental to the health, safety, or general welfare.

SECTION 3. PLANNING COMMISSION ACTIONS.

The Planning Commission hereby approves the Third Revision to Public Use Permit 778 (Project No. 08-0163) subject to the Conditions of Approval attached hereto and incorporated by this reference as Exhibit A.

PASSED, APPROVED AND ADOPTED this 16th day of September 2009.

Robert Devine
Chairman

APPROVED AS TO FORM:

ATTEST:

Thomas Jex
Assistant City Attorney

David Hogan
Planning Commission Secretary

EXHIBIT A
CITY OF WILDOMAR
CONDITIONS OF APPROVAL

Planning Application Number: Public Use Permit 778, Revision 3 (08-0163)	
Project Description: Cornerstone Community Church Parking Area Expansion adding 754 parking spaces, a detention base, replacement athletic fields, athletic field lighting, and related improvements and landscaping.	
Assessor's Parcel Number(s): 367-210-008, 367-210-018, 367-210-034, 367-210-035, 367-210-041, 367-210-043, & 367-140-008	
Approval Date: September 16, 2009	Expiration Date: September 16, 2011

Within 48 Hours of the Approval of This Project

1. The applicant/developer shall deliver to the Planning Department a cashier's check or money order made payable to the County Clerk in the amount of Two Thousand Fifty Seven Dollars (\$2,057.00) which includes the One Thousand Nine Hundred Ninety Three Dollars (\$1,993.00) fee, required by Fish and Game Code Section 711.4(d)(3) plus the Sixty-Four Dollar (\$64.00) County administrative fee, to enable the City to file the Notice of Determination for the Mitigated or Negative Declaration required under Public Resources Code Section 21152 and California Code of Regulations Section 15075. If within said 48-hour period the applicant/developer has not delivered to the Planning Department the check as required above, the approval for the project granted shall be void due to failure of condition [Fish and Game Code Section 711.4(c)].
2. The applicant shall review and sign the Acceptance of Conditions of Approval document that will be provided by the Planning Department staff and return the document with an original signature to the Planning Department.

General Requirements

3. The applicant shall indemnify, protect, defend, and hold harmless, the City, and/or any of its officials, officers, employees, agents, departments, agencies, and instrumentalities thereof, from any and all claims, demands, law suits, writs of mandamus, and other actions and proceedings (whether legal, equitable, declaratory, administrative or adjudicatory in nature), and alternative dispute resolutions procedures (including, but not limited to arbitrations, mediations, and other such procedures), (collectively "Actions"), brought against the City, and/or any of its officials, officers, employees, agents, departments, agencies, and instrumentalities thereof, that challenge, attack, or seek to modify, set aside, void, or annul, the any action of, or any permit or approval issued by, the City

and/or any of its officials, officers, employees, agents, departments, agencies, and instrumentalities thereof (including actions approved by the voters of the City), for or concerning the project, whether such Actions are brought under the California Environmental Quality Act, the Planning and Zoning Law, the Subdivisions Map Act, Code of Civil Procedure Section 1085 or 1094.5, or any other state, federal, or local statute, law, ordinance, rule, regulation, or any decision of a court of competent jurisdiction. It is expressly agreed that the City shall have the right to approve, which approval will not be unreasonably withheld, the legal counsel providing the City's defense, and that applicant shall reimburse City for any costs and expenses directly and necessarily incurred by the City in the course of the defense. City shall promptly notify the applicant of any Action brought and City shall cooperate with applicant in the defense of the Action.

4. The approval of the public use permit shall comply with the provisions of Title 17 – Zoning (Ordinance 348), unless modified by the conditions listed herein. This approval shall expire in two (2) years unless an application for an extension is filed at least 30 days prior to the expiration date. The City, for good cause, may grant up to two (2) one-year extensions of time, one year at a time.
5. The project and all subsequent projects within this site shall comply with all mitigation measures identified in Environmental Assessment 08-0163.
6. The development shall substantially conform to the approved site plan and elevations contained on file with the Planning Department.
7. The developer shall obtain City approval for any modifications or revisions to the approval of this project. Deviations not identified on the plans may not be approved by the City, potentially resulting in the need for the project to be redesigned. Amended entitlement approvals may be necessary as a result.
8. Landscaping installed for the project shall be continuously maintained to the satisfaction of the Planning Director. If it is determined that the landscaping is not being maintained, the Planning Director shall have the authority to require the property owner to bring the landscaping into conformance with the approved landscape plan. The continued maintenance of all landscaped areas shall be the responsibility of the developer or any successors in interest.
9. The Applicant shall dedicate, design and construct all improvement in accordance with City of Wildomar Improvement Plan Check Policies, as further conditioned herein, and Standards and to the satisfaction of The City Engineer.
10. The applicant will be required to provide the following street improvements for this project:
 - A. Design and improve Monte Vista Drive across the project frontage. Improvements shall be in accordance with Standard 109 (Major Frontage Road) with the following modifications; minimum pavement width of 64-

feet with a right-of-way of 91-feet, and a 10-foot wide multi-use trail in addition to the 5-foot wide sidewalk.

- B. Dedicate, design and improve Via Carnaghi Lane from the existing parking lot to Monte Vista Drive. Improvements shall be in accordance with Standard 105 – Section “D” (Local Street) with the following modifications; no curb or AC dike, 32-feet of pavement widened at the intersection with Monte Vista Drive to a minimum of 40-feet of pavement width.
 - C. Design and improve Monte Vista Drive south of the intersection with Via Carnaghi Lane to provide a right turn pocket into Via Carnaghi Lane.
 - D. Gates into the project site shall be located as to provide a minimum of 25 feet of “throat” depth and shall be designed to open into the site to allow a vehicle to completely leave the travel lane(s) when the gate is closed.
11. The applicant shall provide all documentation for the City of Wildomar to accept Via Carnaghi Lane into the street maintenance program.
 12. The applicant shall design and construct/install signing and striping for constructed improvements and any necessary transitions to the satisfaction of the City Engineer. Design shall include, but not be limited to, a stop sign on Via Carnaghi Lane, no stopping signage along one side of Via Carnaghi Lane, and no stopping signage on both sides of Monte Vista.
 13. At all street intersections adjacent to the project, public or private, the Applicant shall install and/or replace street name signs in accordance with the City of Wildomar Standard Details.
 14. The applicant shall dedicate visibility easements for all intersections and driveways per the City of Wildomar Improvement Standards and to the satisfaction of the City Engineer.
 15. The Applicant shall obtain the appropriate clearance letters to the satisfaction of the City Engineer for any sign(s) located within an easement, including a Public Utility Easement.
 16. The applicant shall dedicate a public utility easement adjacent to all public or private streets for overhead and/or underground facilities and appurtenances to the satisfaction of the City Engineer.
 17. The applicant shall install street lights along the streets associated with the development in accordance with the standards of the City of Wildomar Ordinances 460 and 461 and the City of Wildomar procedures.
 18. Erosion control and landscape plans, required for manufactured slopes greater than 3 feet in vertical height, are to be signed by a registered landscape architect and bonded per the requirements of Ordinance 457 (refer to Department Form

284-47). Planting shall occur within 30 days of meeting final grades to minimize erosion and to ensure slope coverage prior to the rainy season.

19. Provide drainage facilities and terracing in conformance with the Uniform Building Code's chapter on "EXCAVATION & GRADING".
20. The applicant shall plant and irrigate all manufactured slopes steeper than a 4:1 (horizontal to vertical) ratio and 3 feet or greater in vertical height with grass or ground cover; slopes 15 feet or greater in vertical height shall be planted with additional shrubs or trees or as approved by the Planning Director and City Engineer.
21. All grading and drainage shall be designed in accordance with the included conditions of approval regarding this application.
22. No grading shall be performed without the prior issuance of a grading permit by the City. All grading shall conform to the California Building Code, Ordinance 457, and all other relevant laws, rules, and regulations governing grading in the City of Wildomar and prior. Prior to commencing any grading which includes 50 or more cubic yards, the applicant shall obtain a grading permit from the Building Department.
23. A slope stability report shall be submitted and approved by the City Engineer for all proposed cut or fill slopes steeper than 2:1 (horizontal to vertical) or over 30 feet in vertical height unless addressed in a previous report.
24. Grading in excess of 199 cubic yards will require performance security to be posted with the City of Wildomar.
25. The minimum drainage grade shall be 1% except on portland cement concrete where 0.35% shall be the minimum.
26. All flood control plans to be reviewed shall be submitted through the City of Wildomar, unless otherwise directed by the City Engineer.
27. All paved off-street parking areas which are conditioned to be paved shall conform to Ordinance 457 base and paving design and inspection requirements.
28. The developer shall take reasonable steps to prevent off-highway vehicles from using the site whenever any portion of the site is used for soil stockpiling purposes. The developer shall secure all parcels on which a stockpile has been placed and shall prevent all off-highway vehicles from using the property.
29. Improvements such as grading, filling, over excavation and re-compaction, and base or paving which require a grading permit are subject to the included Building Department conditions of approval.
30. In order to mitigate any potential impacts to unknown subsurface archaeological resources during grading operations, if an archeological resource is encountered

during grading activities all grading shall be halted or diverted until a qualified archaeologist can assess the resources.

31. Tribal monitors from the Pechanga Tribe shall be allowed to monitor all grading, excavation and groundbreaking activities, including all archaeological surveys, testing, and studies, to be compensated by the developer.
32. If human remains are encountered, State Health and Safety Code Section 7050.5 states that no further disturbance shall occur until the Riverside County Coroner has made the necessary findings as to origin. Further, pursuant to Public Resource Code Section 5097.98(b) remains shall be left in place and free from disturbance until a final decision as to the treatment and disposition has been made. If the Riverside County Coroner determines the remains to be Native American, the Native American Heritage Commission shall be contacted within a reasonable timeframe. Subsequently, the Native American Heritage Commission shall identify the "most likely descendant." The most likely descendant shall then make recommendations and engage in consultation concerning the treatment of the remains as provided in Public Resources Code Section 5097.98.
33. If inadvertent discoveries of subsurface archaeological resources are discovered during grading, the Developer, the project archaeologist, and the appropriate Tribe shall assess the significance of such resources and shall meet and confer regarding the mitigation for such resources. If the Developer and the Tribe cannot agree on the significance or the mitigation for such resources, these issues will be presented to the Planning Director for decision. The Planning Director shall make the determination based on the provisions of the CEQA with respect to archaeological resources and shall take into account the religious beliefs, customs, and practices of the appropriate Tribe.
34. All sacred sites, should they be encountered within the project area, shall be avoided and preserved as the preferred mitigation, if feasible.
35. The landowner agrees to relinquish ownership of all cultural resources, including all archaeological artifacts that are found on the project area, to the Pechanga Tribe for proper treatment and disposition.
36. Blue retro-reflective pavement markers shall be mounted on private streets, public streets and driveways to indicate location of fire hydrants. Prior to installation, placement of markers must be approved by the Riverside County Fire Department.
37. The applicant shall annex into all applicable County Service Areas and Landscaping Maintenance District for landscaping, lighting, drainage and maintenance to the satisfaction of the City Engineer or otherwise form a District where one is not currently in place.
38. The applicant shall design and construct American with Disabilities Act (ADA) access from the public right of way to the main building entrance and van

accessible parking in accordance with all appropriate City of Wildomar Standards and Codes, and ADA requirements and to the satisfaction of the City Engineer and Building Official.

39. The flood control facilities shall be constructed with this project in accordance with applicable standards. The City Engineer shall determine if the facility will be maintained by Flood Control District or the City of Wildomar. The Applicant shall execute a maintenance agreement with the appropriate agency and the City Engineer shall determine if an easement or a parcel is taken in fee title. The plans cannot be signed prior to execution of the agreement.
40. Should this project lie within any assessment/benefit district, the project proponent shall, prior to acceptance of improvements, make application for and pay for their reapportionment of the assessments or pay the unit fees in the benefit district unless said fees are otherwise deferred.
41. The non-security lighting fixtures around the athletic fields shall be turned off at 10:00 pm.
42. The development of the premises shall substantially conform to the approved site plan on file with the Planning Department, unless superseded by these Conditions of Approval.
43. The City, its Planning Director, Planning Commission, and City Council retain and reserve the right and jurisdiction to review and modify this public use permit (including the Conditions of Approval) based on changed circumstances. Changed circumstances include, but are not limited to, a modification of business, a change in scope, emphasis, size or nature of the business, and the expansion, alteration, reconfiguration or change of use. The reservation of right to review any public use permit granted or approved or conditionally approved hereunder by the City, its Planning Director, Planning Commission and City Council is in addition to, and not in-lieu of, the right of the City, its Planning Director, Planning Commission, and City Council to review, revoke or modify any public use permit approved or conditionally approved hereunder for any violations of the conditions imposed on such public use permit or for the maintenance of any nuisance condition or other code violation thereon.
44. This approval shall not be valid until all outstanding permit and application processing fee balances are paid in full. No extensions of time shall be granted unless all balances have been paid in full.

Prior to Issuance of Grading Permit(s)

45. This grading plan involves import or export, prior to obtaining a grading permit, the applicant shall have obtained approval for the import/export location from the City of Wildomar. No grading or haul permit, or phased component thereof, shall be issued until the applicant has obtained approval for the location of any off-site import/export material, as well as the associated haul route(s), for any required

grading from the City Engineering. The applicant's contractor is required to submit for a haul route permit for the hauling of material to and from the project site. Said permit will include limitations of haul hours, number of loads per day, and the posting of traffic control personnel at all approved entrances/exits onto public roads. This permit shall be in place prior to the issuance of the grading permit and the mobilization of equipment on the project site. Route to and from the disposal site(s) may also be subject to additional restrictions. Prior to the issuance of the grading or haul permit, the Planning Director shall review the proposed import sites and haul routes to determine if a new or modified environmental assessment is required. No grading permit shall be issued until any required environmental clearance has been approved by the Planning Director.

46. Geotechnical soils reports, required in order to obtain a grading permit, shall be submitted to the City Engineer for review and approval prior to issuance of grading permit. All grading shall be in conformance with the recommendations of the geotechnical/soils reports as approved by City of Wildomar.
47. Prior to the issuance of a grading permit, it shall be the sole responsibility of the Applicant to obtain any and all easements and/or permissions necessary to perform the grading required for the project. A notarized letter of permission from all affected property owners or easement holders, or encroachment permit, is required for all off-site
48. Prior to issuance of any grading permit or the approval of any improvements plans, whichever comes first, the applicant shall provide the City Engineer with evidence of compliance with the following: "Effective March 10, 2003 owner operators of grading or construction projects are required to comply with the National Pollutant Discharge Elimination System (NPDES) requirement to obtain a construction permit from the State Water Resource Control Board (SWRCB). The permit requirement applies to grading and construction sites of "ONE" acre or larger. The owner operator can comply by submitting a "Notice of Intent", develop and implement a Storm Water Pollution Prevention Plan and a monitoring program and reporting plan for the construction site. For additional information and to obtain a copy of the NPDES State Construction Permit contact the SWRCB at (916) 657-1146. Additionally, at the time the City of Wildomar adopts, as part of any ordinance, new regulations specific to the NPDES, this project shall comply with them.
49. All necessary measures to control dust shall be implemented by the developer during grading to the satisfaction of the City Engineer. A PM10 Control Plan may be required at the time a grading permit is issued.
50. Prior to the issuance of a grading permit, or building permit, whichever occurs first, the developer shall pay all necessary impact and mitigation fees required prior to the issuance of a grading permit. These fees include, but are not limited to, fees associated with the Stephens Kangaroo Rat Habitat Conservation Plan and the Western Riverside Multiple Species Habitat Conservation Plan.

51. Prior to the issuance of a grading permit, the applicant shall submit, and the City approve the Final Water Quality Management Plan which ensures that post-construction flows do not exceed pre-construction levels and that the specified BMPs will minimize any water quality impacts. The Final WQMP include a comprehensive drainage study and plan that includes, but is not limited to: definition with mapping of the existing watersheds; a detailed pre- and post-project hydrologic and hydraulic analysis of the project and project impacts; definition of the local controlling 100-year frequency water levels existing and with project; the proposed method of flow conveyance to mitigate the potential project impacts with adequate supporting calculations; any proposed improvements to mitigate the impacts of increased runoff from the project and any change in runoff; including quality, quantity, volume, and duration in accordance with City of Wildomar's Hydrology Manual, Improvement Standards, and to the satisfaction of the City Engineer. These BMPs shall be consistent with the Final WQMP and installed to the satisfaction of the City Engineer.
52. Prior to the issuance of a grading permit, the developer shall pay the established fee for the Murrieta Creek/Wildomar Valley Area Drainage Plan. Drainage fees shall be paid (with cashier's check or money order only) to the District and a copy of the receipt provided to the City.
53. Prior to the issuance of a grading permit, the developer is required to enter into a Cultural Resources Treatment Agreement with the Pechanga Tribe. This Agreement will address the treatment and disposition of cultural resources and human remains that may be impacted as a result of the development of the project, as well as provisions for tribal monitors.
54. At least 30 days prior to seeking a grading permit, the project applicant shall contact the Pechanga Band of Luiseño Indians (Tribe) to notify the Tribe of grading, excavation and the monitoring program, and to coordinate with the City of Wildomar and the Tribe to develop a Cultural Resources Treatment and Monitoring Agreement. The Agreement shall address the treatment of known cultural resources, the designation, responsibilities, and participation of Native American Tribal monitors during grading, excavation and ground disturbing activities; project grading and development scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and human remains discovered on the site.
55. Prior to the issuance of a grading permit, the developer shall identify the qualified archaeologist to the City who has been retained to evaluate the significance of any inadvertently discovery historical resources. If cultural resources are encountered during ground disturbing activities that were not previously identified, all construction activities will be halted or redirected until a qualified archaeologist can evaluate the nature and significance of the finds and recommend and implement management actions to protect or curate any resources that merit management.

56. Prior to the issuance of a grading permit, the developer shall identify the qualified paleontologist to the City who has been retained to evaluate the significance of any inadvertently discovered paleontological resources. If paleontological resources are encountered during grading or project construction, all work in the area of the find shall cease. The project proponent shall notify the City and retain a qualified paleontologist to investigate the find. The qualified paleontologist shall make recommendations as to the paleontological resource's disposition to the Planning Director. The developer shall pay for all required treatment and storage of the discovered resources.
57. The following requirements shall be included in the Notes Section of the Grading Plan: *"No grubbing/clearing of the site shall occur prior to scheduling the pre-grading meeting with Engineering. All project sites containing suitable habitat for burrowing owls, whether owls were found or not, require a 30-day preconstruction survey that shall be conducted within 30 days prior to ground disturbance to avoid direct take of burrowing owls. If the results of the survey indicate that no burrowing owls are present on-site, then the project may move forward with grading, upon Planning Department approval. If burrowing owls are found to be present or nesting on-site during the preconstruction survey, then the following recommendations must be adhered to: Exclusion and relocation activities may not occur during the breeding season, which is defined as March 1 through August 31, with the following exception: From March 1 through March 15 and from August 1 through August 31 exclusion and relocation activities may take place if it is proven to the City and appropriate regulatory agencies (if any) that egg laying or chick rearing is not taking place. This determination must be made by a qualified biologist."*
58. The following requirement shall be included in the Notes Section of the Grading Plan: *"If at any time during excavation/construction of the site, archaeological/cultural resources, or any artifacts or other objects which reasonably appears to be evidence of cultural or archaeological resource are discovered, the property owner shall immediately advise the City of such and the City shall cause all further excavation or other disturbance of the affected area to immediately cease. The Planning Director at his/her sole discretion may require the property owner to deposit a sum of money it deems reasonably necessary to allow the City to consult and/or authorize an independent, fully qualified specialist to inspect the site at no cost to the City, in order to assess the significance of the find. Upon determining that the discovery is not an archaeological/cultural resource, the Planning Director shall notify the property owner of such determination and shall authorize the resumption of work. Upon determining that the discovery is an archaeological/cultural resource, the Planning Director shall notify the property owner that no further excavation or development may take place until a mitigation plan or other corrective measures have been approved by the Planning Director."*
59. All of the foregoing conditions shall be complied with prior to the issuance of a grading permit.

Prior to Issuance of Building Permit(s)

60. Prior to the issuance of any building permit, the property owner shall obtain a grading permit and/or approval to construct from the Building Department.
61. Prior to the issuance of a building permit, the outdoor lighting for project shall conform with the requirements of Chapter 8.08 of the Wildomar Municipal Code (previously known as Ordinance 655). These items shall be shown on electrical plans submitted prior to the issuance of building permit and shall be reviewed and approved by the Building and Safety Department.
62. Proposed retaining walls will require separate permits. They shall be obtained prior to the issuance of any other building permits unless otherwise approved by the City Engineer. The walls shall be designed by a Registered Civil Engineer unless they conform to the City of Wildomar Standard Retaining Wall designs shown on the Building Department form 284-197.
63. The applicant shall submit landscaping and irrigation plans to Planning Department. These plans shall include water usage calculations, estimate of irrigation, and the location of all existing trees that will remain. All plans and calculations shall be designed to comply City Codes and reflect the comments contained in the September 4, 2009 Memorandum from the City Landscape Architect to the satisfaction of the Planning Director.
64. The applicant shall submit landscaping and irrigation plans within the public right of way to Planning Department. These plans shall include water usage calculations, estimate of irrigation, and the location of all existing trees that will remain. All plans and calculations shall be designed and calculated per the City of Wildomar Road Improvement Standards & Specification, Improvement Plan Check Policies and Guidelines, City Codes and to the satisfaction of the City Engineer.
65. Prior to the issuance of a building permit, the project proponent shall pay fees in accordance with Zone A of the Southwest Area Road and Bridge Benefit District. All fees are based upon the fee schedule in effect at the time of issuance of the permit.
66. All of the foregoing conditions shall be complied with prior to the issuance of a building permit.

Prior to Issuance of Street Improvement Plans

67. Prior to the approval of improvement plans, the developer shall submit to the City Engineer a traffic control plan along Monte Vista Drive to ensure the continued flow of traffic during construction. Improvement plans for the required improvements must be prepared and shall be based upon a design profile extending a minimum of 300 feet beyond the project boundaries at a grade and

alignment as approved by the City Engineer. Completion of road improvements does not imply acceptance for maintenance by the City of Wildomar.

68. Prior to the approval of the street improvement plans, a signing and striping plan is required for this project. The project proponent shall be responsible for any additional paving and/or striping removal caused by the striping plan. to the satisfaction of the City Engineer.

Prior to Final Inspection, Release of Power, or Any Use Allowed by This Permit

For this section, the terms final inspection, release of power, and building occupancy are used interchangeably to signify compliance with all conditions of approval, applicable codes and requirements necessary for the safe and lawful occupation or use of a structure or site.

69. Prior to final inspection, the applicant shall prepare and submit to the Fire Department for approval, a site plan designating required fire lanes with appropriate lane painting and/or signs.
70. Prior to final inspection the applicant shall reconstruct any deteriorated curb, gutter, sidewalk and/or pavement along the project's frontage or along Monte Vista Road to the satisfaction of Public Works. If pavement replacement is required, the Applicant may be required to grind, overlay, and/or slurry seal per City of Wildomar Road Improvement Standards & Specification, Improvement Plan Check Policies and Guidelines and to the satisfaction of Public Works.
71. Prior to the final inspection, all outdoor lighting shall be inspected by the Building and Safety Department to insure compliance with the approved lighting plan and the provisions of Chapter 8.80 of the Wildomar Municipal Code.
72. Electrical power, telephone, communication, street lighting, and cable television lines shall be designed and placed underground in accordance with Ordinance 460 and 461, or as otherwise approved by the City Engineer. The applicant is responsible for coordinating the work with the serving utility company. This also applies to existing overhead lines which are 33.6 kilovolts or less along the project frontage and between the nearest poles offsite in each direction of the project site. A disposition note describing the above shall be reflected on design improvement plans whenever those plans are required. A written proof for initiating the design and/or application of the relocation issued by the utility company shall be submitted to the City Engineer for verification purposes.
73. A separate street light plan is required for this project. Street lighting shall be designed and installed in accordance with City of Wildomar Ordinance 460 and Streetlight Specification Chart found in Specification Section 22 of Ordinance 461. For projects within SCE boundaries use City of Wildomar Ordinance 461, Standard No's 1000 or 1001.

74. Prior to final inspection, all required landscape planting and irrigation shall have been installed consistent with the approved construction plans and shall be in a condition acceptable to the Planning Director. The plants shall be healthy and free of weeds, disease, or pests. The irrigation system shall be properly constructed and in good working order. The applicant shall contact the Planning Department to schedule the final inspection(s).
75. Prior to the final inspection, all outdoor lighting shall be inspected by the Building and Safety Department to insure compliance with the approved lighting plan and the provisions of Chapter 8.80 of the Wildomar Municipal Code.
76. Prior to final inspection the applicant shall install all landscaping in accordance with the approve landscape and irrigation plans.
77. Performance securities, in amounts to be determined by the Planning Director, to guarantee the maintenance of the plantings in accordance with the approved construction landscape and irrigation plan shall be filed with the Planning Department for a period of one year from final Certificate of Occupancy. After that year, if the landscaping and irrigation system have been maintained in a condition satisfactory to the Planning Director, the bond may be released upon request by the applicant.
78. Each parking space reserved for the handicapped shall be identified by a permanently affixed reflectorized sign constructed of porcelain on steel, beaded text or equal, displaying the International Symbol of Accessibility. The sign shall not be smaller than 70 square inches in area and shall be centered at the interior end of the parking space at a minimum height of 80 inches from the bottom of the sign to the parking space finished grade, or centered at a minimum height of 36 inches from the parking space finished grade, ground, or sidewalk. A sign shall also be posted in a conspicuous place, at each entrance to the off-street parking facility, not less than 17 inches by 22 inches, clearly and conspicuously stating the following:

"Unauthorized vehicles parked in designated accessible spaces not displaying distinguishing placards or license plates issued for persons with disabilities may be towed away at owner's expense.
Towed vehicles may be reclaimed by telephoning (951) 245-3300"

In addition to the above requirements, the surface of each parking place shall have a surface identification sign duplicating the Symbol of Accessibility in blue paint of at least three square feet in size.
79. All of the foregoing conditions shall be complied with prior to occupancy or any use allowed by this permit.

OUTSIDE AGENCIES REQUIREMENTS:

80. None.

To: Dave Hogan, Planning Director
From: Laurie Levine, Landscape Architect
Date: September 4, 2009
Subject: Cornerstone Community Church – Conceptual Landscape Plan (08-0163)

The following are my recommendations of adjustments and modifications to be incorporated into the landscape and irrigation construction plans for this project.

1. Incorporate a Bermuda Mix into the athletic field turf area.
2. We can select a street tree of our choice. My suggestions are: Cercidium ‘Desert Museum’, Quercus agrifolia, Quercus ilex, or Olea europaea (fruitless variety).
3. The detention basin must have low water usage shrubs and trees on its slopes in masses. The access road must be constructed of concrete for proper maintenance, due to the steep slope.
4. All plants must be low water usage plants according to WUCOLS III. California native plants should be utilized and mixed with non-native drought tolerant plants where appropriate. All plantings shall be hydro-zoned according to WUCOLS III in the low water use category. The maximum applied water allowance shall not exceed 25 percent of the local annual mean precipitation without a written and graphic statement, included with the landscape design plan, designating the portions of the landscape design for specific recreational purposes.
5. Only California native plants must be utilized on all slopes.
6. The following plants are should not be used: Pinus eldarica, Podocarpus gracilior, Robinia ‘Purple Robe’, Hemerocallis species, Dietes vegeta, Acacia redolens, and Pyracantha species.
7. Boston ivy could be used as a deciduous vine on trash enclosures, but not as the only vine species if there are substantial lengths of retaining walls.
8. Permanent, evergreen shrubs capable of mature growth to between 36 inches and 42 inches high shall be designed to screen the perimeter parking (except where site lines are present, 30 inch maximum).
9. An approved concrete mow strip shall separate all turf and shrub beds.
10. All trash enclosures shall be covered with a wood arbor or other approved architectural treatment to blend with the buildings. All three sides of trash enclosure walls shall have a minimum 5 feet wide planting area (excluding curbing and walkway strip). Flowering vines shall be trained on to the walls. Trees, shrubs and ground covers shall be included

in the planting areas.

11. No light fixture shall be designed for any location in a planting area, which would make it necessary to eliminate a tree. All overhead light fixtures shall be identified on the site plan prior to approval of the site plan.
12. Above ground utility locations shall be approved with prior permission of the Planning Department. Building backflow prevention units shall be located in shrub areas outside of the City right-of-way. All double detector check and backflow assemblies shall be located in shrub beds clearly identified on the grading plan prior to approval of the grading plan. Detector check/backflow devices shall be painted a neutral/earth tone color and any pipes extending above ground shall be finished and painted to match the device. All Edison transformers shall be screened with at least 5 feet of planting located outside the Edison setback (18 inches on sides and rear, 8 feet at the front door).
13. Canopy shade trees shall be located directly adjacent to public parking spaces without walkways or paving between the tree and parking space to provide the required % of parking spaces shaded at 15 years growth. The trees shall be located no farther than 25 to 30 feet on center (depending on the canopy spread of the species) in continuous planting areas. The trees shall be located no farther than 36 feet on center when 6 feet wide curbed tree wells are designed within the parking lot.
14. All plant materials designed for slope areas shall be California native plants, including approved species of trees, shrubs and ground cover. Special consideration to fuel modification requirements shall be considered in the species selection and spacing of plants.
15. All street trees shall be installed from 36-inch box containers at no farther than 30 feet on center. The tree species shall be as directed by the Planning Director.
16. The detention basin shall contain a concrete access and maintenance road at no less than 15 feet wide. The slopes of the basin shall be designed with approved tree and shrub masses, as approved by the Planning Director to mitigate the view of the basin.
17. All shrub planting areas shall be covered with a 2-inch layer of approved bark mulch.
18. All slope planting areas shall be covered with a 4-inch layer of approved bark mulch when planting is completed, designed to stabilize the slope and provide 100% coverage for source control as a best management practice.
19. All fertilizers utilized in pre-planting, post-planting, and long-term maintenance shall be organic or approved slow-release type.
20. All drainage swales shall be designed as 'vegetated swales' with at least 12 inches of soil at the bottom of the swale. The swales shall incorporate the design of low water plant materials that will accommodate incidences of medium to high water as required.

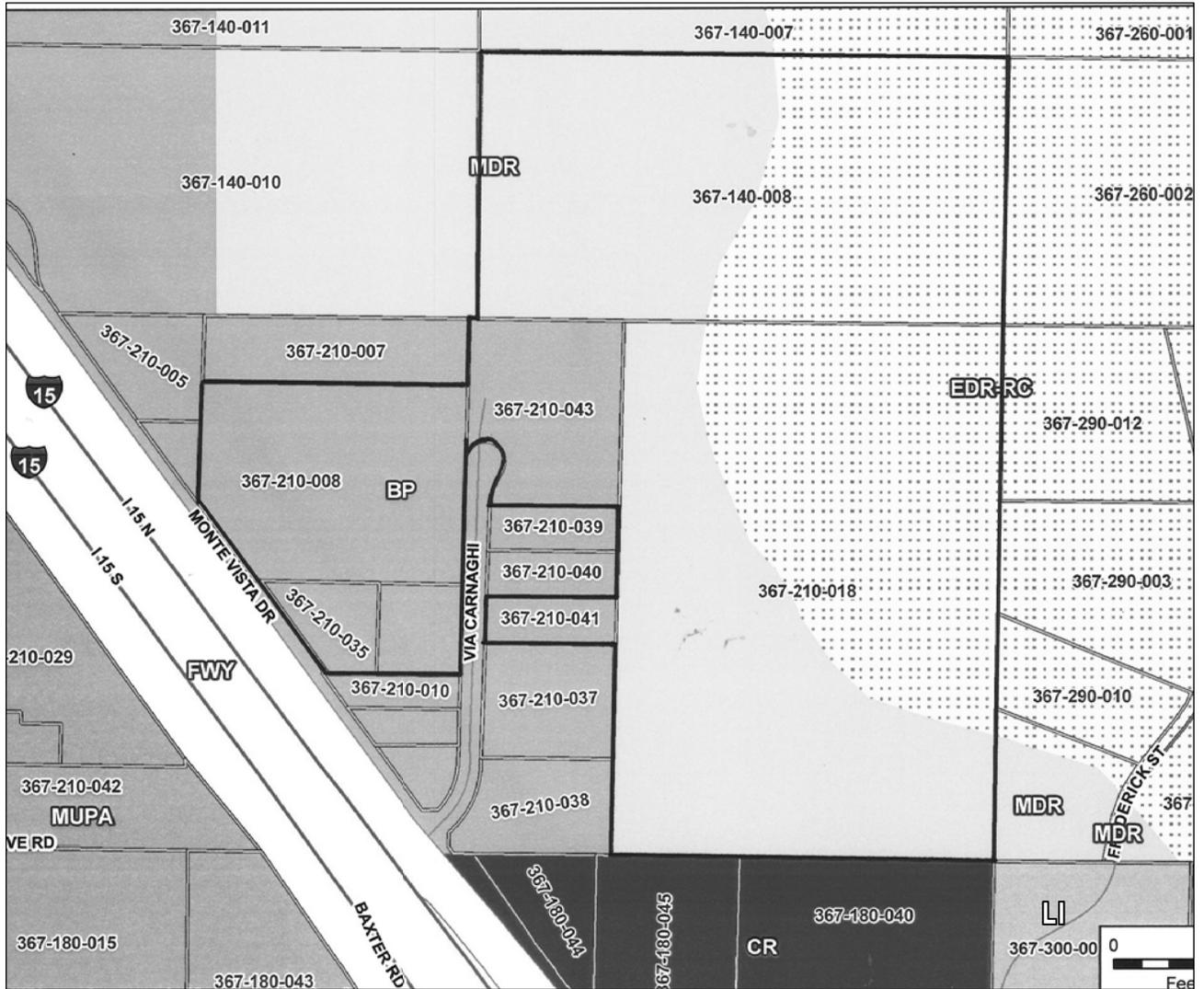
ATTACHMENT C

VICINITY MAP & AERIAL PHOTOGRAPH



ATTACHMENT D

GENERAL PLAN LAND USE MAP



General Plan Land Use Designations:

BP - Business Park

CR - Commercial Retail

EDR - Estate Density Residential (Ranch Community Overlay)

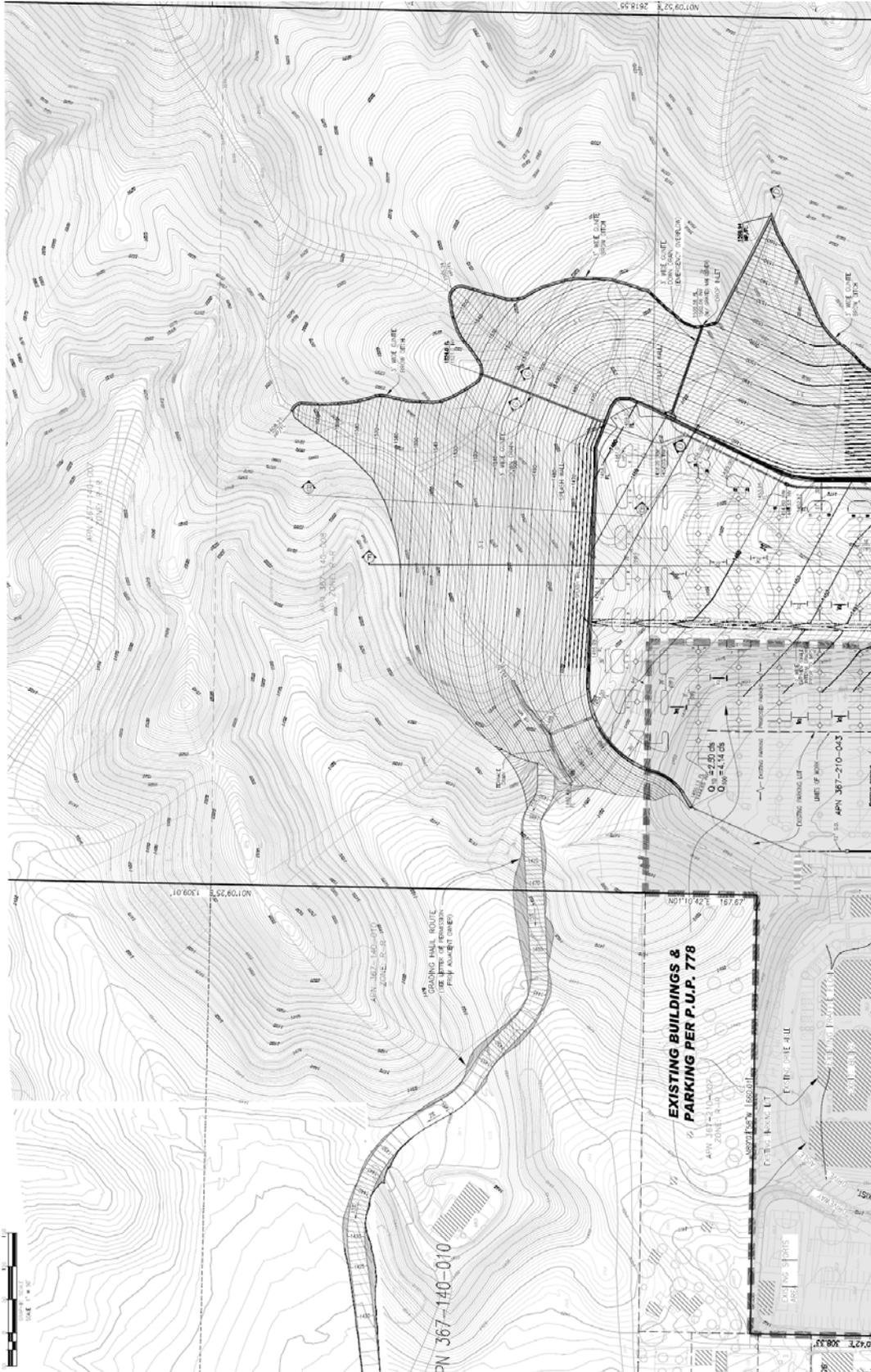
LI - Light Industrial

MDR - Medium Density Residential

ATTACHMENT E

E-2 – SITE PLAN, NORTHERN AREA DETAIL

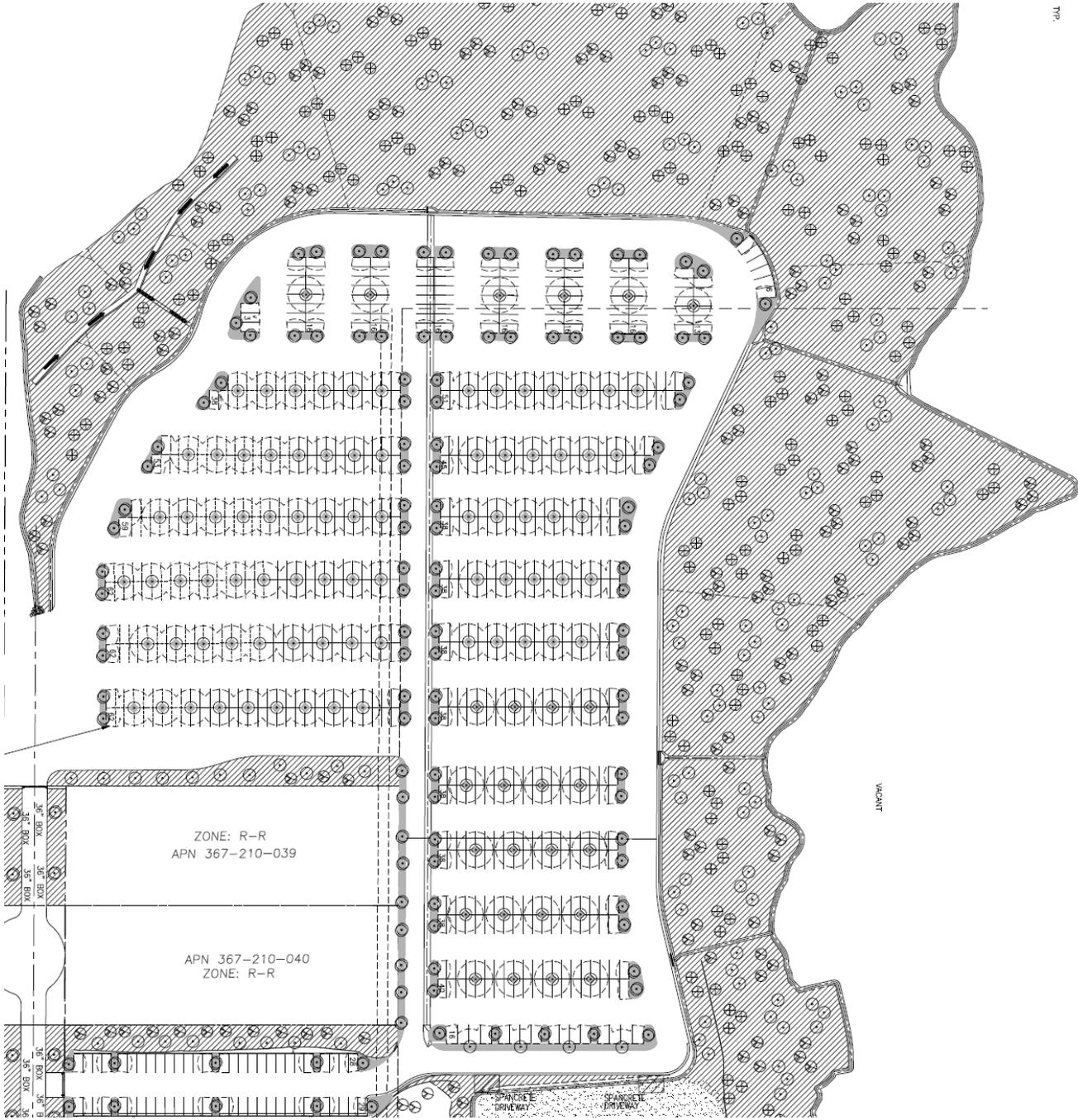
P.U.P. No. 778, REVISED PERMIT No. 3, AMENDED NO. 4



SEE SHEET 3

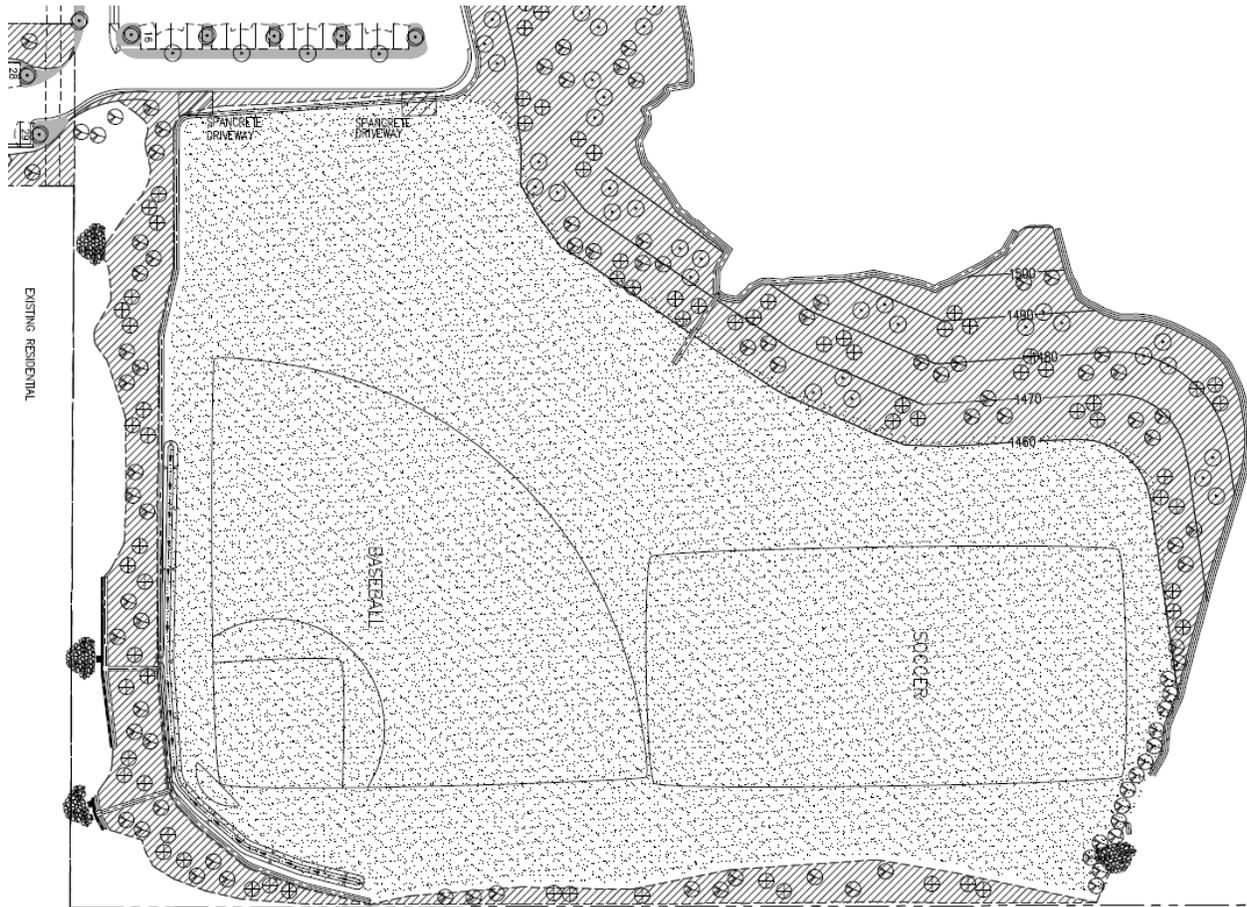
ATTACHMENT F

F-1 CONCEPTUAL LANDSCAPE PLAN, NORTHERN AREA DETAIL



129

F-2 CONCEPTUAL LANDSCAPE PLAN, SOUTHERN AREA DETAIL



SYMBOL	ABBREVIATION	BOTANICAL NAME	COMMON NAME	SIZE
TREES:				
	PIN. ELD.	PINUS ELДАРICA	MONDELL PINE	15 GAL.
	POD. GRA.	PODOCARPUS GRACILIOR	FERN PINE	24"B./36"B.
	ROB. A. 'P.R.'	ROBINIA AMBIGUA 'PURPLE ROBE'	PINK FLOWERING LOCUST	24" BOX
	ACA. STE.	ACACIA STENOPHYLLA	SHOESTRING ACACIA	15 GAL.
PARKING & R.O.W. SHRUBS:				
	HEM. HYB.	HEMEROCALLIS HYBRIDS	DAYLILY	1 GAL
	DIE. VEG.	DIETES VEGETA	FORTNIGHT IRIS	5 GAL
LAWN:				
	B.M. #1	BALLFIELD MIX#1	BERMUDA / RYE MIX	HYDROSEED
SLOPE & R.O.W. SHRUBS, MULCH & GROUNDCOVER:				
	WOOD MULCH	DESIGNER WOOD CHIPS-50/50	COLORLED WOOD MULCH	3" MAX.
	WOOD MULCH	FOREST MULCH	WOOD MULCH	3" MAX.
	ACA. RED.	ACACIA REDOLENS	PROSTRATE ACACIA	1 GAL
	PYR. C. 'L.'	PYRACANTHA COCCINEA 'LELLANDI'	SCARLET FIRETHORN	5 GAL
	MYO. PAR.	MYOPORUM PARVIFOLIUM	PROSTRATE MYOPORUM	1 GAL
VINES:				
NOT SHOWN	PAR. TRI.	PARTENOCCISSUS TRICUSPIDATA	BOSTON IVY	5 GAL

ATTACHMENT G



MUSCO
GREEN GENERATION LIGHTING™

EQUIPMENT LAYOUT

Cornerstone Comm. Church FB/SB
Wildomar, CA

INCLUDES:

- Baseball
- Football

Electrical System Requirements: Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

Installation Requirements: Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.

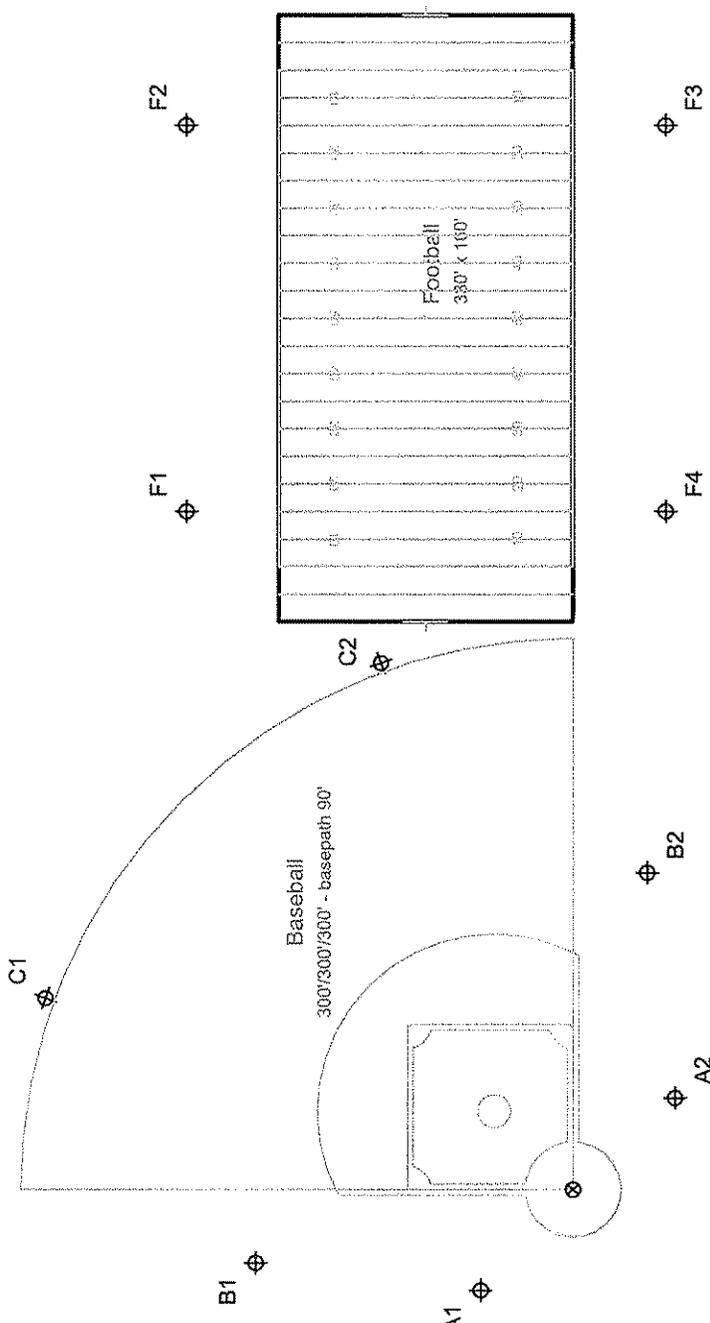
EQUIPMENT LIST FOR AREAS SHOWN

QTY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	LUMINAIRE		QTY/POLE	
					JOCK	1500W MZ		
4	A1-A2	70'	-	70'			5	
2	B1-B2	70'	-	70'			8	
4	F1-F4	70'	-	70'			6	
10	← TOTALS →							60

SINGLE LUMINAIRE AMPERAGE DRAW CHART

Ballast Specifications (50 min power factor) Line Amperage Per Luminaire (max draw)

Single Phase Voltage	120 (ea)	208 (ea)	240 (ea)	277 (ea)	347 (ea)	380 (ea)	415 (ea)	480 (ea)
1500 watt MZ	15.0	8.6	7.7	7.5	6.5	5.1	4.7	3.7



By: Josh Hawk

File #: 139881

Date: 21-Jul-09

Pole location(s) with dimensions are relative to 0,0 reference point(s)

SCALE IN FEET 1 : 100





GUARANTEED PERFORMANCE

ILLUMINATION SUMMARY

Baseball

Cornerstone Comm. Church FB/SB
Wildomar, CA

Baseball

- Size: 300'x300'x300' - basepath 90'
- Grid Spacing = 30.0' x 30.0'
- Values given at 3.0' above grade

- Luminaire Type: Green Generation
- Rated Lamp Life: 5,000 hours
- Avg Lumens/Lamp: 134,000

**CONSTANT ILLUMINATION
HORIZONTAL FOOTCANDLES**

No. of Target Points:	Infield	Outfield
Average:	25	73
Maximum:	50.26	31.86
Minimum:	57	44
Avg/Min:	38	23
Max/Min:	1.31	1.39
UG (Adjacent Pts):	1.48	1.33
CV:	1.31	1.43
	0.11	0.16
Average Lamp Tilt Factor:	1.000	
Number of Luminaires:	36	
Avg KW over 5,000 hours:	56.3	
Max KW:	61.2	

Guaranteed Performance: The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.

Field Measurements: Averages shall be +/-10% in accordance with IESNA RP-6-01 and CIBSE LG4. Individual measurements may vary from computer predictions.

Electrical System Requirements: Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

Installation Requirements: Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.

By: Josh Hawk

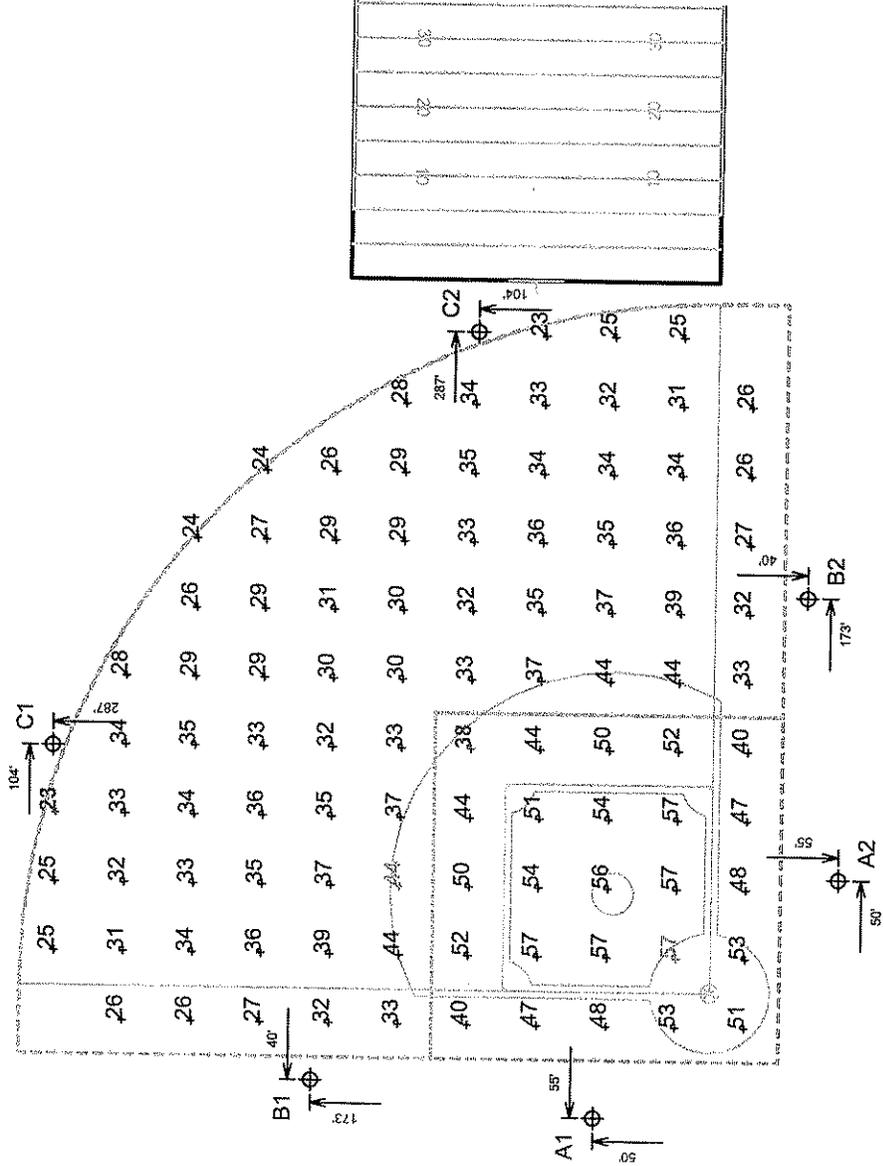
File #: 139881

Date: 21-Jul-09

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Print Date (21/Jul/2009) & Time (14:18)

EQUIPMENT LIST FOR AREAS SHOWN							
Pole			Luminaires				
QTY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	LAMP TYPE	OTHER GRID	
4	A1-A2	70'		70'	1500W MZ	5	
2	C1-C2	70'		70'	1500W MZ	8	
6	B1-B2	70'		70'	1500W MZ	8	
TOTALS						36	36



SCALE IN FEET 1 : 80



Pole location(s) with dimensions are relative to 0,0 reference point(s)





GUARANTEED PERFORMANCE

ILLUMINATION SUMMARY

Football

Cornerstone Comm. Church FB/SB
Wildomar, CA

Football

- Size: 330' x 160'
- Grid Spacing = 30.0' x 30.0'
- Values given at 3.0' above grade

- Luminaire Type: Green Generation
- Rated Lamp Life: 5,000 hours
- Avg Lumens/Lamp: 134,000

**CONSTANT ILLUMINATION
HORIZONTAL FOOTCANDLES**

No. of Target Points: 66
 Entire Grid
 Average: 31.67
 Maximum: 37
 Minimum: 26
 Avg/Min: 1.24
 Max/Min: 1.47
 UG (Adjacent Pts): 1.44
 CV: 0.11
 Average Lamp Tilt Factor: 1.000
 Number of Luminaires: 24
 Avg KW over 5,000 hours: 37.54
 Max KW: 40.8

Guaranteed Performance: The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.

Field Measurements: Averages shall be +/-10% in accordance with IESNA RP-6-01 and CIBSE LG4. Individual measurements may vary from computer predictions.

Electrical System Requirements: Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

Installation Requirements: Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.

By: Josh Hawk

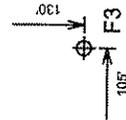
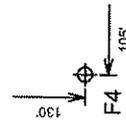
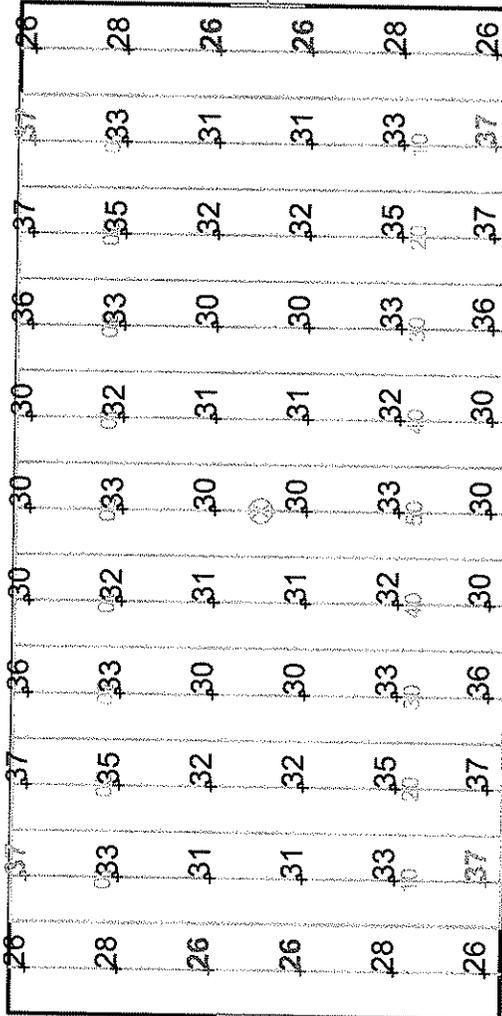
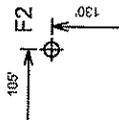
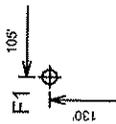
File #: 139881

Date: 21-Jul-09

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EQUIPMENT LIST FOR AREAS SHOWN

Pole		Luminaires							
QTY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	LAMP TYPE	QTY / POLE	THIS GRID	OTHER GRIDS	
4	F1-F4	70'		70'	1500W MZ	6	6	0	
4	TOTALS						24	24	0



SCALE IN FEET 1 : 60



Pole location(s) dimensions are relative to 0,0 reference point(s)





ILLUMINATION SUMMARY

Interstate Spill

Comerstone Comm. Church FB/5B
Wildomar, CA

Interstate Spill

- Grid Spacing = 30.0'
- Values given at 3.0' above grade

- Luminaire Type: Green Generation
- Rated Lamp Life: 5,000 hours
- Avg Lumens/Lamp: 134,000

CONSTANT ILLUMINATION

HORIZONTAL FOOTCANDLES

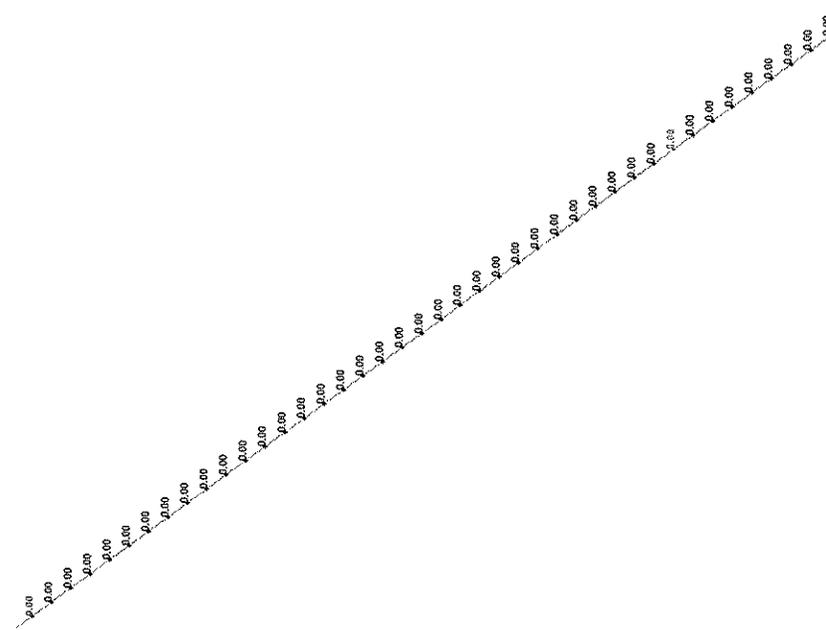
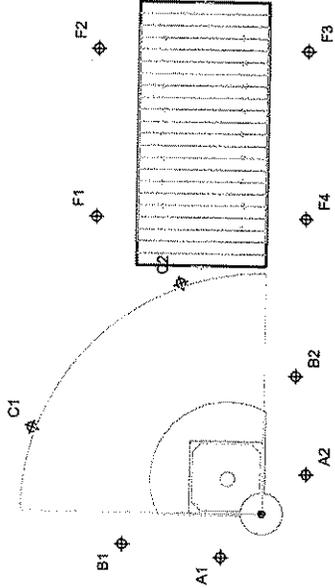
No. of Target Points:	Entire Grid	
Average:	0.0015	1,000
Maximum:	0.00	60
Minimum:	0.00	93.84
Average Lamp Tilt Factor:		102.0
Number of Luminaires:		
Avg KW over 5,000 hours:		
Max KW:		

Guaranteed Performance: The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.

Field Measurements: Averages shall be +/-10% in accordance with IESNA RP-6-01 and CBSE LG4. Individual measurements may vary from computer predictions.

Electrical System Requirements: Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

Installation Requirements: Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.



EQUIPMENT LIST FOR AREAS SHOWN

QTY	LOCATION	SIZE	WARRANTY	QTY/	THE
			PER	FILE	ORDER
			YEAR	NO.	NO.
4	A1-A2	70'	1500W/12	5	0
2	B1-B2	70'	1500W/12	8	0
4	F1-F4	70'	1500W/12	8	0
10	TOTALS			60	0



By: Josh Hawk
File #: 138981
Date: 21-Jul-09

Field location(s) dimensions are relative to 0.0 reference point(s)



MUSCO
GREEN GENERATION LIGHTING™

GUARANTEED PERFORMANCE

EQUIPMENT LIST FOR AREAS SHOWN

QTY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	LAMP TYPE	Luminaires		
						THIS GRID	OTHER GRID	GRIDS
4	A1-A2	70"	-	70"	1500W MZ	5	5	0
2	B1-B2	70"	-	70"	1500W MZ	8	8	0
6	TOTALS					36	36	0

ILLUMINATION SUMMARY

Baseball
Cornerstone Comm. Church FB/SB
Wildomar, CA

150' Baseball Spill
• Grid Spacing = 30.0'
• Values given at 3.0' above grade

• Luminaire Type: Green Generation
• Rated Lamp Life: 5,000 hours
• Avg Lumens/Lamp: 134,000

CONSTANT ILLUMINATION HORIZONTAL FOOTCANDLES

Entire Grid
No. of Target Points: 74
Average: 0.0737
Maximum: 0.15
Minimum: 0.03

Average Lamp Tilt Factor: 1.000
Number of Luminaires: 36
Avg KW over 5,000 hours: 56.3
Max KW: 61.2

Guaranteed Performance: The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.

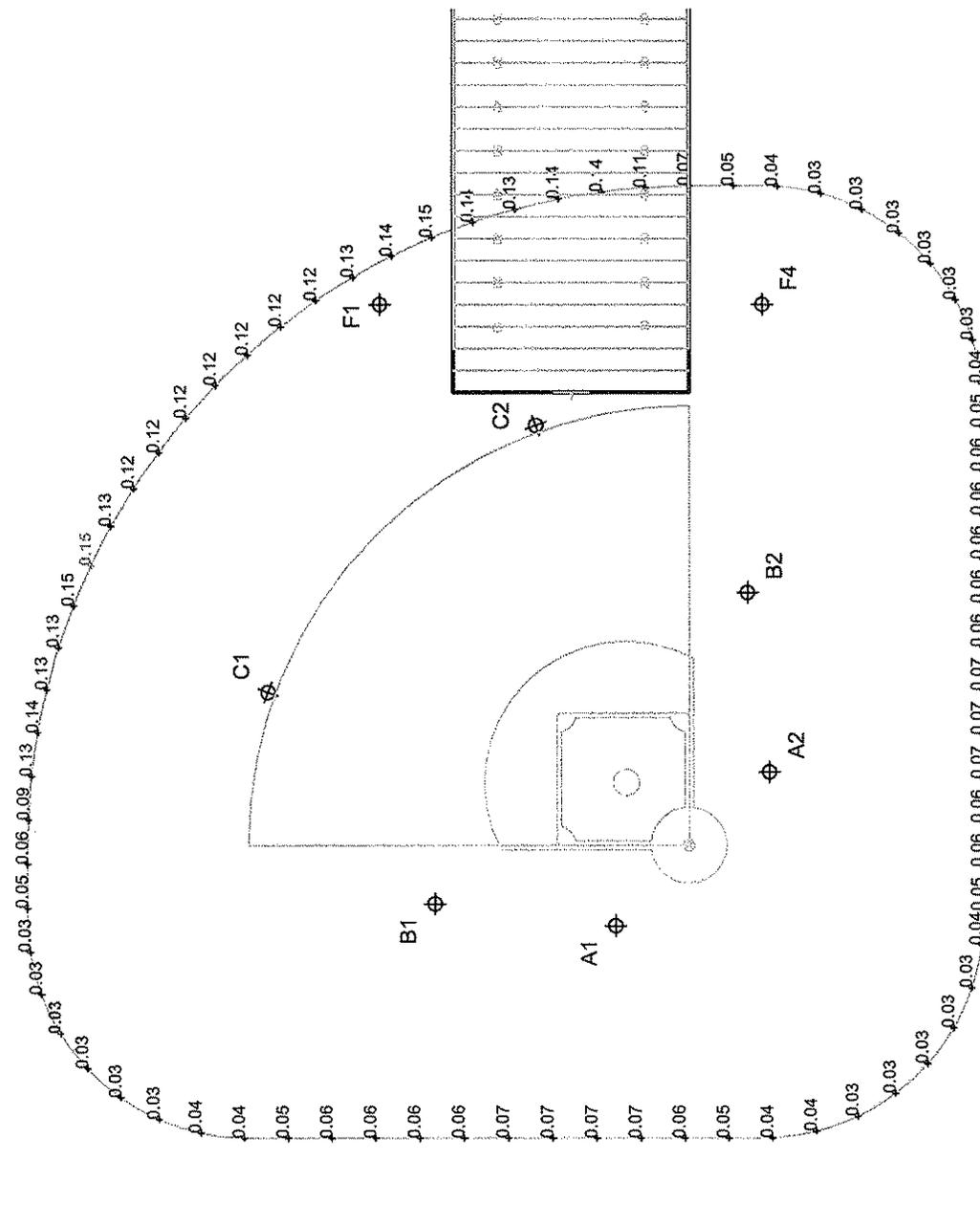
Field Measurements: Averages shall be +/-10% in accordance with IESNA RP-6-01 and CIBSE LG4. Individual measurements may vary from computer predictions.

Electrical System Requirements: Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

Installation Requirements: Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.

By: Josh Hawk
File #: 139881
Date: 21-Jul-09

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Pole location(s) with dimensions are relative to 0,0 reference point(s)

SCALE IN FEET 1 : 120





GUARANTEED PERFORMANCE

ILLUMINATION SUMMARY

Baseball

Cornerstone Comm. Church FB/SB
Wildomar, CA

150' Baseball Spill

- Grid Spacing = 30.0'
- Values given at 3.0' above grade

- Luminaire Type: Green Generation
- Rated Lamp Life: 5,000 hours
- Avg Lumens/Lamp: 134,000

**CONSTANT ILLUMINATION
MAX VERTICAL FOOTCANDLES**

No. of Target Points:	74
Average:	0.3556
Maximum:	0.74
Minimum:	0.17
Average Lamp Tilt Factor:	1.000
Number of Luminaires:	36
Avg KW over 5,000 hours:	56.3
Max KW:	61.2

Guaranteed Performance: The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.

Field Measurements: Averages shall be +/-10% in accordance with IESNA RP-6-01 and CIBSE LG4. Individual measurements may vary from computer predictions.

Electrical System Requirements: Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

Installation Requirements: Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.

By: Josh Hawk

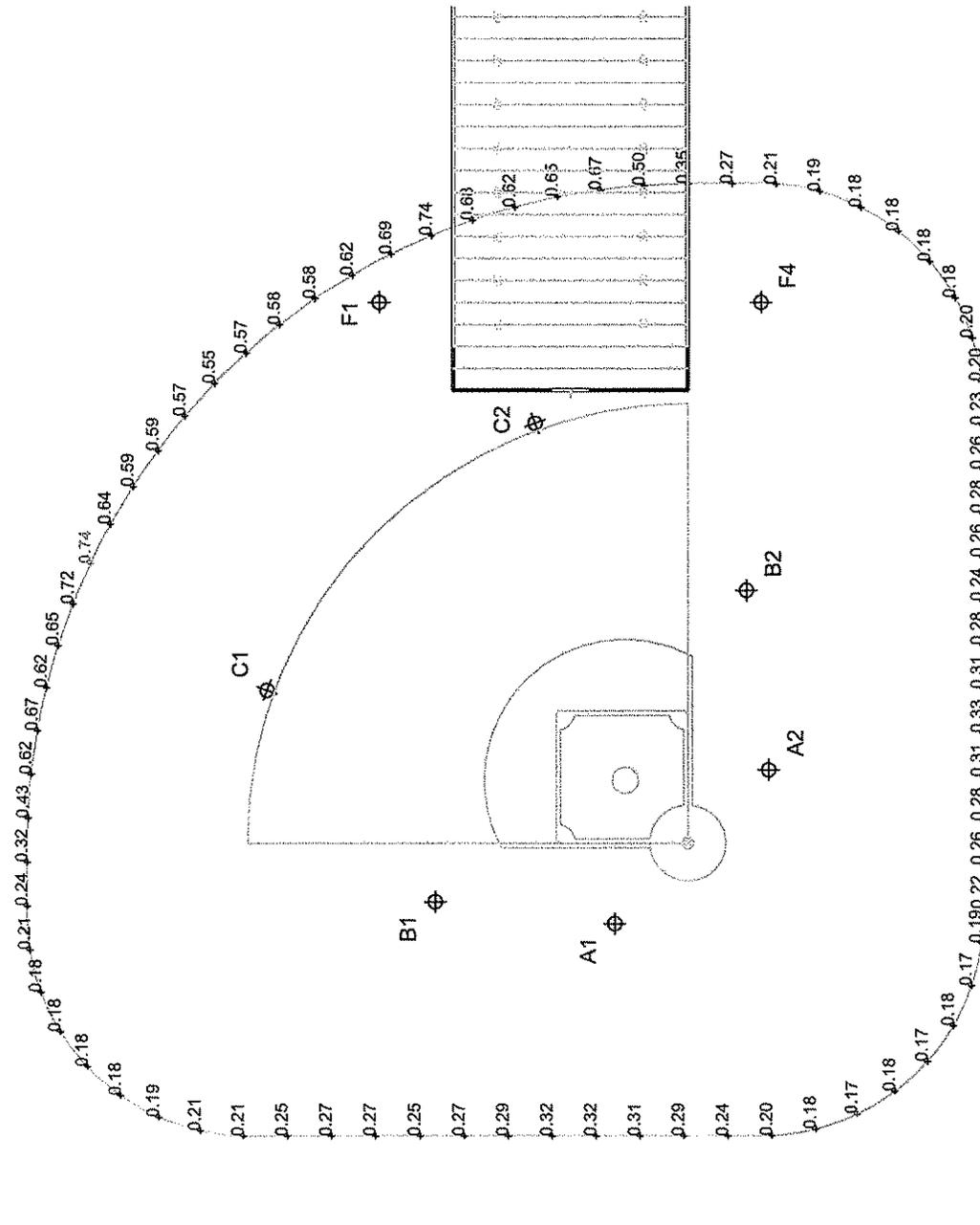
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Date: 21-Jul-09

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EQUIPMENT LIST FOR AREAS SHOWN								
Pole		Luminaires						
QTY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	LAMP TYPE	QTY / POLE	THIS GRID	OTHER GRID
4	A1-A2	70"	-	70'	1500W MZ	5	5	0
2	C1-C2	70"	-	70'	1500W MZ	8	8	0
6	B1-B2	70"	-	70'	1500W MZ	36	36	0
		TOTALS						



SCALE IN FEET 1 : 120





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GUARANTEED PERFORMANCE

ILLUMINATION SUMMARY

Football

Cornerstone Comm. Church FB/SB
Wildomar, CA

150' Football Spill

- Grid Spacing = 30.0'
- Values given at 3.0' above grade

- Luminaire Type: Green Generation
- Rated Lamp Life: 5,000 hours
- Avg Lumens/Lamp: 134,000

CONSTANT ILLUMINATION HORIZONTAL FOOTCANDLES

No. of Target Points: 64
 Entire Grid
 Average: 0.0621
 Maximum: 0.10
 Minimum: 0.04
 Average Lamp Tilt Factor: 1.000
 Number of Luminaires: 24
 Avg KW over 5,000 hours: 37.54
 Max KW: 40.8

Guaranteed Performance: The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.

Field Measurements: Averages shall be +/-10% in accordance with IESNA RP-6-01 and CIBSE LG4. Individual measurements may vary from computer predictions.

Electrical System Requirements: Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

Installation Requirements: Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.

By: Josh Hawk

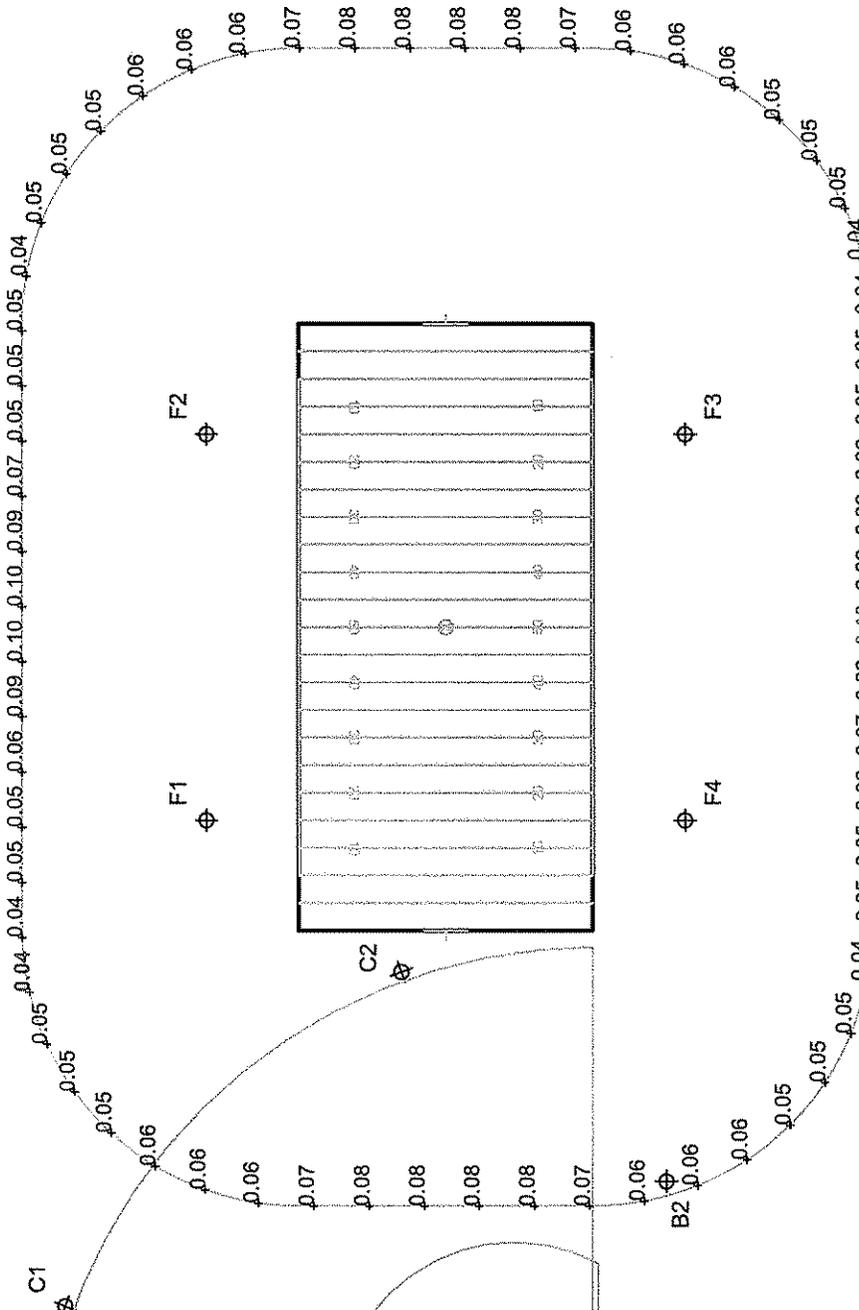
File #: 139881

Date: 21-Jul-09

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EQUIPMENT LIST FOR AREAS SHOWN							
Pole		Luminaires					
QTY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	LAMP TYPE	OTHER GRID GRDS.	
4	F1-F4	70"	-	70'	1500W MZ	0	
4	TOTALS					24	0



Pole location(s) with dimensions are relative to 0,0 reference point(s)



MUSCO
GREEN GENERATION LIGHTING™

GUARANTEED PERFORMANCE

EQUIPMENT LIST FOR AREAS SHOWN

CITY	LOCATION	SIZE	GRADE ELEVATION	MOUNTING HEIGHT	Luminaires					
					LAMP TYPE	QTY / POLE	THIS GRID / OTHER GRIDS			
4	F1-F4	70'	-	1500W MZ	6	6	0			
4	TOTALS							24	24	0

ILLUMINATION SUMMARY

Football
Cornerstone Comm. Church FB/SB
Wildomar, CA

150' Football Spill

- Grid Spacing = 30.0'
- Values given at 3.0' above grade

- Luminaire Type: Green Generation
- Rated Lamp Life: 5,000 hours
- Avg Lumens/Lamp: 134,000

CONSTANT ILLUMINATION MAX VERTICAL FOOTCANDLES

Entire Grid: 64

No. of Target Points: 64

Average: 0.2770

Maximum: 0.37

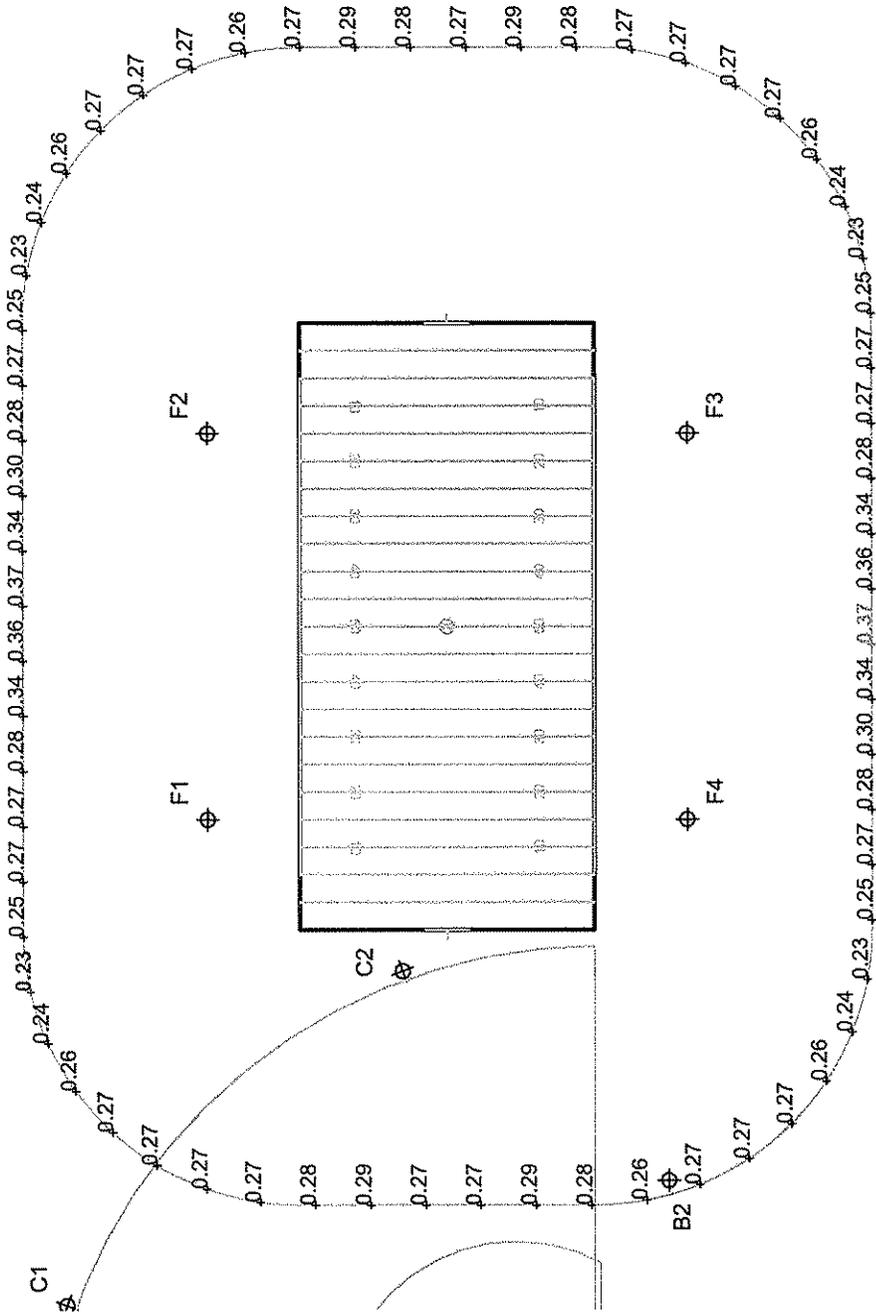
Minimum: 0.23

Average Lamp Tilt Factor: 1.000

Number of Luminaires: 24

Avg KW over 5,000 hours: 37.54

Max KW: 40.8



Guaranteed Performance: The CONSTANT ILLUMINATION described above is guaranteed for the rated life of the lamp.

Field Measurements: Averages shall be +/-10% in accordance with IESNA RP-6-01 and CIBSE LG4. Individual measurements may vary from computer predictions.

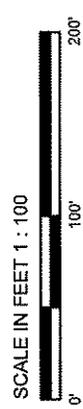
Electrical System Requirements: Refer to Amperage Draw Chart and/or the "Musco Control System Summary" for electrical sizing.

Installation Requirements: Results assume +/- 3% nominal voltage at line side of the ballast and structures located within 3 feet (1m) of design locations.

By: Josh Hawk
File #: 139881
Date: 21-Jul-09

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Pole location(s) with dimensions are relative to 0,0 reference point(s)



ATTACHMENT H

INITIAL STUDY

FOR

CORNERSTONE COMMUNITY CHURCH

PARKING LOT EXPANSION

Project Number 08-0163
(PUBLIC USE PERMIT 778, REVISION NO. 3)

City of Wildomar
Planning Department
City of Wildomar
23873 Clinton Keith Road, Suite 201
Wildomar, CA 92595

August 2009

TABLE OF CONTENTS

I.	PROJECT INFORMATION	1
II.	APPLICABLE GENERAL PLAN LAND USE POLICIES AND ZONING	5
III.	ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED	5
IV.	DETERMINATION	6
V.	ENVIRONMENTAL ISSUES ASSESSMENT	8
	Aesthetics	
	1. Scenic Resources.....	8
	2. Mt. Palomar Observatory.....	9
	3. Other Lighting Issues.....	10
	Agriculture Resources	
	4. Agriculture.....	12
	Air Quality	
	5. Air Quality Impacts.....	13
	Biological Resources	
	6. Wildlife & Vegetation.....	20
	Cultural Resources	
	7. Historic Resources.....	22
	8. Archaeological Resources.....	23
	9. Paleontological Resources.....	24
	Geology and Soils	
	10. Alquist-Priolo Earthquake Fault Zone or County Fault Hazard Zones.....	25
	11. Liquefaction Potential Zone.....	26
	12. Groundshaking Zone.....	26
	13. Landslide Risk.....	27
	14. Ground Subsidence.....	27
	15. Other Geologic Hazards.....	27
	16. Slopes.....	28
	17. Soils.....	29
	18. Erosion.....	30
	19. Wind Erosion and Blowsand from project either on or offsite.....	30
	Hazards and Hazardous Materials	
	20. Hazards and Hazardous Materials.....	31
	21. Airports.....	32
	22. Hazardous Fire Area.....	33
	Hydrology and Water Quality	
	23. Water Quality Impacts.....	33
	24. Floodplains.....	36
	Land Use/Planning	
	25. Land Use.....	37
	26. Planning.....	37

TABLE OF CONTENTS (continued)

Mineral Resources	
27. Mineral Resources	39
Noise	
28. Airport Noise	40
29. Railroad Noise	40
30. Highway Noise	41
31. Other Noise	41
32. Noise Effects on or by the Project	41
Population and Housing	
33. Housing	43
Public Services	
34. Fire Services	44
35. Sheriff Services	44
36. Schools	45
37. Libraries	45
38. Health Services	46
Recreation	
39. Parks and Recreation	46
40. Recreational Trails	46
Transportation / Traffic	
41. Circulation	47
42. Bike Trails	50
Utility and Service Systems	
43. Water	51
44. Sewer	51
45. Solid Waste	52
46. Utilities	52
Other	
47. Other	53
Mandatory Findings of Significance	
48-51	54
Summary of Mitigation Measures	56
VI. REFERENCES	60

TABLE OF CONTENTS (continued)

TABLES

Table 5.1	Construction Thresholds.....	15
Table 5.2	Significance Thresholds	16
Table 5.3	Alternative Dirt-Hauling Scenarios	16
Table 41.1	Intersection Analysis for Existing Conditions	48

FIGURES (at end of document)

Figure 1	Regional Location Map
Figure 2	Vicinity Map
Figure 3	Site Development Plan Overview
Figure 4	Site Plan, Northern Area Detail
Figure 5	Site Plan, Southern Area Detail
Figure 6	Site Photographs

**CITY OF WILDOMAR
ENVIRONMENTAL ASSESSMENT FORM: INITIAL STUDY**

Environmental Assessment (E.A.) Number: PL08-0163

Project Case Type (s) and Number(s): Cornerstone Community Church Parking Lot and Athletic Fields P.U.P. 778 Revised Permit Rev. No. 3, Amendment 4

Lead Agency Name: City of Wildomar Planning Department

Address: 23873 Clinton Keith Road, Suite 201, Wildomar, CA 92595

Contact Person: David Hogan, Planning Director

Telephone Number: 951-677-7751

Applicant's Name: Cornerstone Community Church

Applicant's Address: 34570 Monte Vista Drive, Wildomar, CA 92595

I. PROJECT INFORMATION

A. Project Description

The proposed project is the revision of an existing Public Use Permit (PUP) 778, Revision 3. The purpose of the project is to expand the parking capacity and relocate the existing ball fields at a developed church site. The proposed project disturbance area is located on a site of approximately 83.01 acres at 34570 Monte Vista Drive and is generally east of Interstate 15, and north of Baxter Road in Section 26 of Township 6 South, Range 4 West of the USGS – Wildomar, California Quadrangle, 7.5 Minute Series topographic map, San Bernardino Base and Meridian in the City of Wildomar, California. The latitude and longitude of the site is 33° 37' 06"N and 113° 15' 45"W. The regional location and vicinity maps are shown on Figures 1 and 2, respectively.

The site is partially developed with a church campus on 83.01 acres, which contains a church, a Kindergarten through 12th Grade school, athletic fields and a parking lot which currently has 383 parking spaces (376 standard and 7 handicap parking spaces). Approximately one-third of the site will remain undeveloped as a result of this proposal. The proposed project would result in the construction of 764 additional parking spaces, a baseball diamond, soccer field, and drainage improvements on approximately 24.42 acres. The sports fields/recreation area would encompass approximately 6.25 acres of the site and approximately 10.09 acres of the site would be graded slopes. The Site Development Plan is shown on Figures 3, 4, and 5.

Approximately 47.48 acres of the site would remain unimproved. Drainage facilities would include vegetated enhanced bio-swales for stormwater quality improvement. Improvements to Via Carnaghi Lane in accordance with City conditions of approval would also be included in the proposed project. The relocated sports fields, with lighting, baseball backstop, soccer goals and other related sports equipment, would provide recreational areas for church members and the existing private school, no public use of the sports facility is expected. The project includes roadway improvements to Via Caneghi Lane from the site south to Monte Vista Road. Via Carneghi Lane is currently only partially improved.

In order to construct the parking area and sports fields, approximately 700,000 cubic yards of material will need to be exported from the site to various sites in the surrounding area. The applicant is proposing to construct a temporary haul route from the current upper parking lot to Monte Vista Road to avoid impacting Via Carneghi Lane. Only about 600 feet of the 1,500-foot long haul road will be involve new construction because most of the proposed off-site haul road is located on an unpaved driveway on the adjacent property. The exported materials will be hauled via Monte Vista Road to either Bundy Canyon Road in the north or Baxter Road in the south. From

there the haul trips will access the area and regional road network. During construction, there would be up to 116 round trips per day to export dirt from the site during construction. Additional traffic from construction workers and delivery of materials would also occur during the implementation of the proposed project.

Site Development

As noted above, the project disturbance area encompasses approximately 24.42 acres of an 83.01-acre site. The project proposes that 24.42 acres will be graded. Estimates indicate that up to 700,000 cubic yards of material will be needed to be disposed of to prepare the site for the improvements based on the preliminary grading plans. The following equipment is expected to be onsite during rough grading and construction at the site:

D8N Caterpillar Dozer	2 each (One dozer during extended hauling phase)
980G Loader	2 each (One loader during extended hauling phase)
4,000 Gallon Water Truck	1 each
Trucks (14 cu yd hauling capacity)	50 each

Grading and construction activities are projected to take place over a period of 24 to 25 months and would require up to 116 loads of fill dirt per day to be disposed of off-site for a total of ±50,000 loads over approximately 436 days. If the duration of grading activities is extended to address regional air quality concerns, the project duration could be longer.

At time there is one tentatively identified fill disposal site located in the nearby City of Menifee (near the intersection of Lindenburger & Simpson) that can accept approximately 300,000 cubic yards of dirt. The remaining 400,000 cubic yards of dirt would be deposited at one or more sites yet to be identified. All disposal sites are expected to be within 10 miles of the site. Dirt disposal would take place over approximately 22 months based on a 5-day a week operation. Weather conditions, such as rain or excessive winds, may prolong the period of dirt disposal as would additional trip reductions required to ensure compliance with regional air quality significance thresholds.

Construction

The project proponent indicates that construction of improvements will require approximately 3 to 4 months to complete after excess soil material is removed from the site.

- Phase 1: Rough Grading of the entire project footprint (may be simultaneous with dirt removal). Includes an initial excavation and hauling sub-phase and extended hauling sub-phase.
- Phase 2: Fine/precise grading for, and construction of, the parking lots, athletic fields, lighting, and drainage improvements as illustrated by the proposed revised P.U.P. No. 778.

Actual implementation of the development phases may overlap as work progresses.

Occupancy

The proposed plot plan would result in an additional 764 parking spaces to support the existing church and related school uses. No new buildings are proposed as part of this revision to Public Use Permit 778.

B. Type of Project: Site Specific ; Countywide ; Community ; Policy

C. Total Project Area: Approximately 24.42 acres on a 83.01-acre site

Residential Acres: N/A **Lots:** N/A **Units:** N/A **Projected No. of Residents:** N/A

Commercial Acres: N/A **Lots:** N/A **Sq. Ft. of Bldg. Area:** N/A **Est. No. of Employees:** N/A

Industrial Acres: N/A **Lots:** N/A **Sq. Ft. of Bldg. Area:** N/A **Est. No. of Employees:** N/A

Other: Parking lot expansion (an additional 764 parking spaces), replacement athletic fields, and drainage/water quality improvements.

D. Assessor's Parcel Nos: 367-210-008, 367-210-018, 367-210-034, 367-210-035, 367-210-041, 367-210-043, 367-140-008, and 367-140-010.

E. Street References: 34570 Monte Vista Drive; near the intersection with Via Carnaghi Lane, east of Interstate-15 and north of Baxter Road.

F. Section, Township & Range Description or reference/attach a Legal Description:
Section 26 of Township 6 South, Range 4 West of the USGS Topographic Map, 7.5 Minute Series, Wildomar, California Quadrangle, San Bernardino Base and Meridian.

G. Brief description of the existing environmental setting of the project site and its surroundings:

The project site is located in the City of Wildomar, Riverside County, California. Regional access to the area is provided in a north-south direction by the Interstate 15 (I-15) and 215 (I-215) freeways, and State Highways 74 (SH 74) and 79 (SH 79) in an east-west direction.

The proposed project is located on approximately 24.42-acres at 34570 Monte Vista Drive and is generally east of Interstate 15, and north of Baxter Road in Section 26 of Township 6 South, Range 4 West of the USGS – Wildomar, California Quadrangle, 7.5 Minute Series topographic map, San Bernardino Base and Meridian. It is located at the northern edge of the Murrieta Creek drainage area in the Santa Margarita River Watershed. Stormwater on the site drains south/southwesterly towards Murrieta Creek. Murrieta Creek drains into the Santa Margarita River which flows into the Pacific Ocean through Camp Pendleton Marine Corps Base in northern San Diego County. Photographs of the existing site are contained in Figure 6.

The project region is characterized by varied topography with Lake Elsinore to the northwest, the Santa Ana Mountains and the floodplain of Murrieta Creek to the west, and the Sedco Hills to the north and east. The Santa Ana Mountains form a portion of the northern peninsular ranges of the Perris Block bounded by the San Jacinto fault zone to the northeast, Elsinore fault zone to the southwest, and the Cucamonga fault zone to the north. The site is located in a seismically active area typical of southern California and is likely to experience ground shaking due to earthquakes on nearby faults. The site is not within, but is near, an Alquist Priolo Study Zone. The closest Alquist Priolo Study Zone, the Elsinore Fault Zone is located approximately 2.2 miles southwest of the proposed project site. The maximum credible earthquake for the Elsinore-Temecula Fault Zone is

6.8 on the Richter Scale. The site is not considered subject to liquefaction or subsidence. (EnGen, 2006)

The site topography ranges from steep sided hills in the northern and northeastern part of the site to gently rolling hills in the southern portion of the site. Elevations range from approximately $\pm 1,420$ feet above mean sea level at the southwestern corner to $\pm 1,692$ feet above mean sea level at the center of the eastern boundary of the proposed project site. Soils at the site belong to the Cajalco-Temescal-Las Posas Association. Eight soil types have been mapped on the site: Cajalco rocky fine sandy loam (15 to 50 percent slopes, eroded); Cienega rocky sandy loam (15 to 50 percent slopes, eroded); Gorgonio loamy sand (channeled, 2 to 5 percent slope); Hanford coarse sandy loam (2 to 8 percent slope); Hanford coarse sandy loam (8 to 5 percent slope); Monserate sandy loam (8 to 15 percent slope, eroded); Terrace escarpments; and Vista coarse sandy loam (8 to 15 percent slopes, eroded). The soils have a very low expansion potential.

Natural watercourses are not present on the proposed project site. An earthen berm was constructed in the southwestern portion of the site to channel storm water into the flood control facilities located within and along Monte Verde Drive. A drainage channel has developed along that berm. (Principie, 2007)

The site has been regularly disced as part of weed control measures. Vegetation at the site includes non-native grassland, Riversidean sage scrub, and coastal sage-chaparral scrub. (Principie, 2007)

The proposed project site is not in or near a Criteria Cell, cell group, or sub-unit in the Elsinore Plan Area of the MSHCP. The nearest criteria cell is located approximately 2,295 feet south of the nearest cell. The site is located within the burrowing owl survey area and was surveyed for burrowing owl on May 17, 2008. No habitat for the owl or evidence of its presence, now or within the recent past, was found on site or in close proximity to the site. (Principie, 2007) The project is within the range of the Stephens kangaroo rat and is within the Stephens Kangaroo Rat Habitat Conservation Plan Fee Area. California gnatcatchers were observed on the site in 2001 and 2004. Take of gnatcatchers on the site is authorized under the MSHCP.

No recorded paleontological sites exist on the project site. Part of the site is considered in an area of high paleontological sensitivity by the County of Riverside. No archaeological resources were found on site during the field survey, but there is a potential for resources to be found during ground disturbance. No historical resources were found on the project site.

The proposed project does not have an Agriculture Preserve designation. However, the previously developed portions of the site were considered Farmland of Local Importance.

According to the General Plan, the site is not in a Flood Zone or Special Flood Area. The project is within Flood Zone C, an area of minimal flooding. The site is not within a high fire zone. However, the project is within the Palomar Lighting Ordinance Zone B which has special nighttime lighting requirements. The proposed project is not within an Airport Influence Area.

The project site is within 1,000 feet of Interstate 15 and the ambient (existing background) noise levels are dominated by vehicular traffic noise.

The proposed project is within the Lake Elsinore Unified School District. The project is served by Southern California Edison, Elsinore Valley Municipal Water District, Comcast Cable, and Southern California Gas Company. The church campus is not connected to sewer, but has an on-site septic system.

II. APPLICABLE GENERAL PLAN LAND USE POLICIES AND ZONING

A. General Plan Designation(s): Business Park (BP) and Medium Density Residential (MDR)

B. Land Use Planning Area (L.U.P.A.) Information

1. **Subarea, if any:** N/A
2. **Community Policy Area, if any:** N/A

C. Area Plan Land Use Allocation Map Information

1. **Area Plan, if any:** Elsinore Area Plan
2. **Area Plan Land Use Designation, if any:** Business Park (BP) and Medium Density Residential (MDR)

D. Adopted Specific Plan Information

1. **Name and Number of Specific Plan, if any:** N/A
2. **Specific Plan Planning Area, and Policies, if any:** N/A

E. Existing Zoning: R-R (Rural Residential)

F. Proposed Zoning, if any: No change

G. Adjacent and Surrounding Zoning:

North: Commercial-Office (C-O), Single Family (R-1), Open Space Combining (R-5)
 South: Rural Residential (R-R)
 East: Rural Residential (R-R), Scenic Highway Commercial (C-P-S)
 West: Rural Residential (R-R) and Interstate I-15

H. Other Agencies Whose Approval, Consultation, or Permitting is Required:

None.

III. ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below (■) would be potentially affected by this project, involving at least one impact that is a “Potentially Significant Impact” or “Less than Significant with Mitigation Incorporated” as indicated by the checklist on the following pages.

<input checked="" type="checkbox"/> Aesthetics	<input checked="" type="checkbox"/> Hazards & Hazardous Materials	<input type="checkbox"/> Public Services
<input type="checkbox"/> Agriculture Resources	<input checked="" type="checkbox"/> Hydrology/Water Quality	<input type="checkbox"/> Recreation
<input checked="" type="checkbox"/> Air Quality	<input type="checkbox"/> Land Use/Planning	<input checked="" type="checkbox"/> Transportation/Traffic
<input type="checkbox"/> Biological Resources	<input type="checkbox"/> Mineral Resources	<input type="checkbox"/> Utilities/Service Systems
<input checked="" type="checkbox"/> Cultural Resources	<input checked="" type="checkbox"/> Noise	<input type="checkbox"/> Other
<input checked="" type="checkbox"/> Geology/Soils	<input type="checkbox"/> Population/Housing	<input checked="" type="checkbox"/> Mandatory Findings of Significance

IV. DETERMINATION

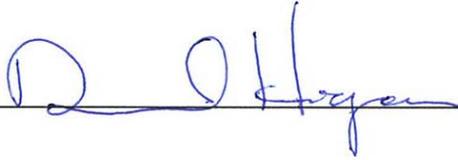
On the basis of this initial evaluation:

A PREVIOUS ENVIRONMENTAL IMPACT REPORT/NEGATIVE DECLARATION WAS NOT PREPARED	
<input type="checkbox"/>	I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
<input checked="" type="checkbox"/>	I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project, described in this document, have been made or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
<input type="checkbox"/>	I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.

A PREVIOUS ENVIRONMENTAL IMPACT REPORT/NEGATIVE DECLARATION WAS PREPARED	
<input type="checkbox"/>	I find that although the proposed project could have a significant effect on the environment, NO NEW ENVIRONMENTAL DOCUMENTATION IS REQUIRED because (a) all potentially significant effects of the proposed project have been adequately analyzed in an earlier EIR or Negative Declaration pursuant to applicable legal standards, (b) all potentially significant effects of the proposed project have been avoided or mitigated pursuant to that earlier EIR or Negative Declaration, (c) the proposed project will not result in any new significant environmental effects not identified in the earlier EIR or Negative Declaration, (d) the proposed project will not substantially increase the severity of the environmental effects identified in the earlier EIR or Negative Declaration, (e) no considerably different mitigation measures have been identified and (f) no mitigation measures found infeasible have become feasible.
<input type="checkbox"/>	I find that although all potentially significant effects have been adequately analyzed in an earlier EIR or Negative Declaration pursuant to applicable legal standards, some changes or additions are necessary but none of the conditions described in California Code of Regulations, Section 15162 exist. An ADDENDUM to a previously-certified EIR or Negative Declaration has been prepared and will be considered by the approving body or bodies.
<input type="checkbox"/>	I find that at least one of the conditions described in California Code of Regulations, Section 15162 exist, but I further find that only minor additions or changes are necessary to make the previous EIR adequately apply to the project in the changed situation; therefore a SUPPLEMENT TO THE ENVIRONMENTAL IMPACT REPORT is required that need only contain the information necessary to make the previous EIR adequate for the project as revised.

I find that at least one of the following conditions described in California Code of Regulations, Section 15162, exist and a **SUBSEQUENT ENVIRONMENTAL IMPACT REPORT** is required: (1) Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; (2) Substantial changes have occurred with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or (3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any the following: (A) The project will have one or more significant effects not discussed in the previous EIR or negative declaration; (B) Significant effects previously examined will be substantially more severe than shown in the previous EIR or negative declaration; (C) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible, and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measures or alternatives; or (D) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR or negative declaration would substantially reduce one or more significant effects of the project on the environment, but the project proponents decline to adopt the mitigation measures or alternatives.

Signature



Date

8/21/09

David Hogan, Planning Director
Printed Name

V. ENVIRONMENTAL ISSUES ASSESSMENT

In accordance with the California Environmental Quality Act (CEQA) (Public Resources Code Section 21000 - 21178.1), this Initial Study has been prepared to analyze the proposed project to determine any potential significant impacts upon the environment that would result from construction and implementation of the project. In accordance with California Code of Regulations, Section 15063, this Initial Study is a preliminary analysis prepared by the Lead Agency, the City of Wildomar, in consultation with other jurisdictional agencies, to determine whether a Negative Declaration, Mitigated Negative Declaration, or an Environmental Impact Report is required for the proposed project. The purpose of this Initial Study is to inform the decision-makers, affected agencies, and the public of potential environmental impacts associated with the implementation of the proposed project.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
AESTHETICS - Would the project:				
1. Scenic Resources				
a) Have a substantial effect upon a scenic highway corridor within which it is located?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings and unique or landmark features; obstruct any prominent scenic vista or view open to the public; or result in the creation of an aesthetically offensive site open to public view?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Site Line Photographs (Markham Development Management Group, Inc.)

Findings of Fact:

- a. The project site is located adjacent to Interstate 15 which is not designated as a State or County scenic highway, but is eligible for listing as a State scenic highway. Therefore, implementation of the proposed project must comply with the applicable policies in the General Plan and not diminish the value of the viewshed to surrounding properties. The current views of the property from Interstate 15 consist of the existing multi-story church building and the mature landscaping. Due to the nature of the proposed project (parking lot and athletic fields), and the proposed location of the parking lot and fields behind the existing structures, the project is not anticipated to have a significant effect upon a scenic highway corridor. No buildings are proposed to be constructed as part of the project. The only structures proposed with this project are associated with the athletic fields and would likely include fencing and benches for observers, backstop and lighting structures and would not interfere with long-range views of the local mountains. All of these items would be located against the back drop of the hills located along the eastern edge of the property. As a result, any impacts are expected to be less than significant.
- b. The proposed project site consists of a relatively level area at the base of a row of the Lower Sedco Hills a pronounced series of hills on the east side of Interstate 15 between Bundy Canyon Road and Baxter Road. The main north south ridge located along the eastern property line is the primary visual component of the project site and is not being altered by the proposed parking lot expansion. The primary community views of the site are from the southwest (see the images located in Figure 5). However because the proposed backstop and lighting structures are located at the base of the Lower Sedco hills between existing developments, the overall visual impact is expected to be less than significant. From the

other directions, the views are partially concealed by the existing church and school buildings and by the existing project landscaping which will block most of the views of the proposed site changes. The views from the north and east are obscured by the existing ridgelines. Views from the west are generally blocked by the existing church facility and landscaping along Monte Vista Road. Views from the southeast are blocked by existing commercial development. The backstops and lighting for the proposed athletic fields will be visible from the area near the Baxter Road overcrossing. The exception to this is the athletic field lighting which, when in use, will be generally visible over the buildings and landscaping (from the west and south). Because these facilities are not opaque the hillside backdrop will remain visible from the community. In addition, the main ridgeline will continue tower over these improvements. In some cases, the upper graded areas along the ridgeline slopes will be visible from across the valley as well as from any of the existing or future residential properties located on the surrounding hills. However, when the slope areas are revegetated, the visual impacts are expected to minimal since the graded areas will largely blend into the surrounding hillsides.

An existing single family unit located on the ridgeline to the east would also experience a change in their westerly views as a result the project. The view from this hilltop has continued to change of the years as vacant properties have been developed. The incremental change from undeveloped rural/vacant to suburban continues to occur citywide and is envisioned to occur in the General Plan. The unit is located approximately 80 feet above the proposed athletic fields. At the present time, the westerly view is of the rolling hills covered with native vegetation between the residence and the developed areas along Monte Vista Road and Interstate 15. The project will grade and lower the elevation of these areas to install two athletic fields. The primary change will be the shift in color from tans and browns to turf green with the development of the project. None of the proposed parking areas will be located near this residence. As a result, the overall impact of the project is expected to be less than significant. Please see the discussion under Other Lighting Issues for an analysis of the potential lighting impacts.

While the project will change the visual character of the area by developing a vacant property, the development will not substantially damage scenic resources nor obstruct any prominent scenic vista or view open to the public; or result in the creation of an aesthetically offensive site open to public view. Consequently, any impacts are considered a less than significant impact.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
2. Mt. Palomar Observatory Interfere with the night time use of the Mt. Palomar Observatory, as protected through Riverside County Ordinance No. 655?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Elsinore Area Plan, Chapter 8.80 of the Wildomar Municipal Code, previously identified as Ordinance 655 Mount Palomar Lighting Ordinance, and Musco Lighting Survey, July 2009

Findings of Fact: According to the Riverside County Comprehensive General Plan, the project site is located within the designated 30-mile Special Lighting Area that surrounds the Mt. Palomar Observatory. Chapter 8.80 of the Wildomar Municipal Code (an ordinance regulating light pollution) was adopted by the County Board of Supervisors on June 7, 1998 and went into effect on July 7, 1998. The intent of Chapter 8.80 is to restrict the permitted use of certain light fixtures emitting into the night sky undesirable light which have a detrimental effect on astronomical observation and research. Chapter 8.80 contains approved materials and methods of installation, definitions, general requirements, requirements for lamp source and shielding, prohibitions and exceptions. Generally, parking lot lighting is required to be low pressure sodium fixtures that are consistent with the requirements of protecting the continued use of the Mount Palomar Observatory.

The proposed project includes limited parking lot security lighting and athletic field lighting, but no other permanent or temporary lighting. The project proposes ten lighting fixtures to illuminate the athletic fields, six for the baseball field and four for the soccer field. The proposed project lighting would comply with the requirements of Chapter 8.80 of the Wildomar Municipal Code. Because compliance with Chapter 8.80 would occur, no specific mitigation measure is required to mitigate project impacts to a less than significant level. Outdoor lighting that conforms to Chapter 8.80 shall be shown on electrical plans submitted prior to the issuance of building permit and shall be reviewed and approved by the Building and Safety Department. Prior to final building inspection, outdoor lighting shall be inspected by the Building and Safety Department to insure compliance with the approved lighting plan.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
3. Other Lighting Issues				
a) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Expose residential property to unacceptable light levels?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Ordinance No. 655, and Musco Lighting Survey, July 2009, International Dark Sky Organization (Outdoor Lighting Code Handbook Version 1.14, December 2000 / September 2002)

Findings of Fact:

a-b. The proposed project includes limited parking lot lighting and athletic field lighting, but no other permanent or temporary lighting. The project proposes ten lighting fixtures to illuminate the athletic fields, six for the baseball field and four for the soccer field. The light poles are anticipated to be approximately 70 feet high and will each contain 1500 W MZ lamps. The lights will be focused and the fields and screened to reduce off-site light spillage. The lighting study determined that the average illumination in the infield will range from 38 to 57 footcandles in the infield of the baseball diamond and 23 to 39 footcandles in the outfield. According to the lighting study, the anticipated illumination levels approximately 150 feet from the outside edge of the ballfield were all less than fourth of a one foot candle. For the soccer field, the illumination levels ranged from 37 to 26 footcandles. The illumination levels 150 feet from the edge of the soccer field were also all less than one-fourth of a footcandle. According to the lighting study, the anticipated illumination levels approximately 150 feet from the light poles (still within the boundaries of the subject property) were all less than tenth of a one foot

candle. According to the International Dark Sky Organization, recommended lighting levels for social or recreational sports should range between 20 to 50 footcandles. The slightly higher lighting levels on portions of the infield are not expected to be significant because the higher illuminated area is fairly small and with the light shielding and because of the adjacent landscaping. The parking lot illumination will be at a much lower illumination level necessary to meet exterior security lighting requirements. Typical parking lot illumination is generally less than five footcandles.

The nearest residences are approximately 300 feet east of the proposed soccer fields and 300 feet west of the proposed baseball diamond. Both of these residences will be slightly effected by the athletic field lighting whenever the lights are on. However, the lights will generally be turned off by 10:00 pm. This distances and limited hours of operation are expected to minimize any potential impacts. The residence east of the soccer field is located approximately 80 feet about the elevation of the proposed field. This puts the proposed lighting at an elevation approximately ten feet lower than the floor elevation of the existing residence. Since the lighting fixtures are located below this unit, will be shielded to prevent off-site light spillage, will not be on seven days a week, and are located over 400 feet from the fixtures, no significant impacts are anticipated. The residence adjacent to Via Carnaghi is slightly below the elevation of the proposed ballfield. The back of the unit is approximately 300 feet from the ballfield. As previously demonstrated, off-field light spillage a distance of 150 feet is less than one-quarter of a footcandle. Because of the distance and the existing landscaping the lighting impacts to this residence are not expected to be significant. Finally there is another residence approximately 700 feet northwest of the parking lot expansion. However, this third residence is behind a hill and over 1,500 feet from the athletic fields and will not be effected by the proposed project lighting. Some of the project lighting may also be briefly visible by drivers traveling northbound along Interstate 15. For the southbound drivers, the lights will be much less visible because the existing church buildings.

The project proposing to site the fields in such that the activities will be directed north and away from the residence(s), and will be shielded to minimize the effects on freeway traffic, thereby reducing the potential for spill-over from the project site. Nearby residential areas shall be shielded from lighting by design (shielding, directional or low energy lighting) or by visual buffering. The applicant shall submit a precise lighting plan demonstrating that exterior night light shall spill into adjacent residential properties. This can be achieved through the use of fully shielded luminaries suitable for sports-lighting, which can reduce off-field spill and up-lighting. The final lighting plan shall be reviewed and approved by the Planning Department, prior to the issuance of Building Permits. This plan shall illustrate how lighting and landscaping combine to minimize light and glare impacts to the residences located near the project site.

The proposed mitigation measures will reduce the potential impacts on nearby residences to less than significant. With the design of the proposed project lighting, compliance with the requirements of Chapter 8.80, potential light and glare impacts to the nearby residents should be less than significant. However, to ensure that exterior lighting from project does not produce light and glare affecting adjacent uses, the following mitigation measure will be implemented. Overall, through the design of the lighting and implementation of mitigation measures the proposed project can reduce impacts associated with spillage and uplighting that can affect nighttime views in the proposed project area.

Mitigation: The following mitigation measures shall be implemented in order to reduce project impacts:

- 3-1 The plans for the athletic field lighting and parking area security lighting shall be reviewed and approved by the Planning and Building and Safety Department prior to approval of the building permit.**
- 3-2 Prior to final inspection, the outdoor lighting shall be inspected by the Building and Safety and Planning Departments to insure compliance with the approved lighting plan and ensure that potential off-site light spillage is minimized. An additional pre-field use inspection may also be required by the Planning Director. Additional lighting adjustments may be required at this time.**
- 3-3 Prior to final inspection, the project landscaping shall be installed in accordance with the approved plans and reviewed by the Planning Department.**

Monitoring: The above described mitigation measures will be verified during final inspection process. Inspection notes verifying implementation of the mitigation measures shall be retained in the project file.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
AGRICULTURE RESOURCES - Would the project:				
4. Agriculture				
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland) as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Conflict with existing agricultural use, or a Williamson Act (agricultural preserve) contract (Riverside County Agricultural Land Conservation Contract Maps)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Cause development of non-agricultural uses within 300 feet of agriculturally zoned property (Chapter 5.40 of the Wildomar Municipal Code, previously identified as Ordinance 625 Right-to-Farm)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Figure OS-2.

Findings of Fact: The project site is vacant and undeveloped. It is located adjacent to a church campus and is in an area transitioning from rural residential uses to business, institutional, commercial and medium density residential uses. It is located near the major transportation corridor, Interstate-15. No significant agricultural uses occur within the immediate vicinity of the project site.

- a. The proposed project site is not located in an area designated as Prime Farmland, Unique Farmland, or Farmland of Statewide Importance. However, the site is located in an area designated as farmland of local importance, but is not located within an agricultural preserve. Agricultural uses are gradually being phased out of the project area due to the extent of surrounding existing and proposed development. Additionally, the proposed project site has varying topographical relief, which is unsuitable for most agricultural uses. The combination of surrounding uses and hilly terrain would make it unlikely that the site would be considered

for agricultural uses in the future. Impacts associated with the conversion of agricultural resources from the proposed project is less than significant.

- b. The proposed project site is undeveloped, is not under cultivation and would not conflict with existing agricultural use, or a Williamson Act (agricultural preserve) contract (Riverside County Agricultural Land Conservation Contract Maps). The proposed project is not located within an agricultural preserve and is not near any agricultural operations. Therefore, no impacts are associated with conflicting existing agricultural uses from the implementation of the proposed project.
- c. Zoning within the proposed project site's vicinity is designated; C-O (Commercial-Office) and R-1 (single family) to the north; RR (Rural Residential) to the south; RR (Rural Residential) to the east and CPS (Scenic Highway Commercial) to the west. All of these land use designation are non-agricultural. Therefore, the project would not result in development of non-agricultural uses within 300 feet of agriculturally zoned property. No impact will occur.
- d. It is not anticipated that the proposed project will involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to non-agricultural uses. Therefore, this is a less than significant impact.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
AIR QUALITY - Would the project:				
5. Air Quality Impacts				
a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d) Expose sensitive receptors which are located within 1 mile of the project site to project substantial point source emissions?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e) Involve the construction of a sensitive receptor located within one mile of an existing substantial point source emitter?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: South Coast Air Quality Management District's "CEQA Air Quality Handbook" (CEQA Handbook), Air Quality Impact Analysis, Air Quality Impact Analysis Cornerstone Church Expansion, Giroux and Associates, May 27, 2008

Findings of Fact: An air quality analysis was prepared by Giroux and Associates in May 2008 to determine the impacts of the proposed project. Operational impacts are considered less than significant and do not require mitigation. Most operational air quality emissions are from mobile sources such as automobiles. The proposed development is projected to generate a small amount

of traffic. Upon completion, the expanded athletic facilities and parking are anticipated to generate 30 additional a.m. trips during the morning peak hour and 88 additional p.m. trips during the afternoon peak hour. Therefore, based on the limited trip generation operational air quality impacts from project-related traffic are considered less than significant.

Approximately 24 acres of the proposed project will be graded. The new parking lot will be approximately seven acres and approximately 17 acres will become manufactured slopes and a new baseball field and soccer field. It is anticipated that approximately 700,000 cubic yards of excess soil will be generated for the development of the parking lot, baseball and soccer fields and flood control improvements. The California Air Resources Board (CARB) computer model URBEMIS2007 assumes that one-fourth of the total disturbance area will be disturbed simultaneously to determine fugitive dust emissions calculations.

Air quality impacts of constructing the sports fields and parking lot expansion could be considered significant without mitigation due to the amount of dirt disturbed and disposed of off-site. As part of grading activities, the number of truckloads of dirt that would be moved from the site and transported for fill at another location would be limited to a maximum of 116 twenty-mile round trips or the equivalent. This scenario, with mitigation incorporated as described in detail in the air quality analysis, would result in air quality that would be less than significant.

Various grading and hauling scenarios were examined in terms of air quality impacts. Intense grading and hauling activity resulted in exceeding thresholds for Oxides of Nitrogen (NO_x). In order to remain below the significance threshold for Oxides of Nitrogen, it was determined that a maximum number of daily 20-mile round trips is 85 for the most intense period of grading and 116 for the remainder of the grading and hauling period. As trip lengths may vary according to exact disposal site, and unforeseen circumstances such as change in disposal site may occur, a table showing different scenarios that stay below the air quality thresholds was developed as a guide for use during project implementation please refer to Table 5.4. The proposed project will be limited by total mileage and number of trips.

a-c. Appendix G of the current State CEQA Guidelines indicates that a project has a significant effect on air quality if the project violates any ambient air quality standard, contributes substantially to an existing air quality violation, or exposes sensitive receptors to substantial pollutant concentrations. In this instance the proposed project, if approved, would result in additional parking areas and sports fields at an existing church site. Existing and proposed uses at this location are consistent with the existing and anticipated uses at the church campus and the land uses with the project vicinity. As a result, this project would not have a potential to create a significant conflict with implementation of the adopted Air Quality Management Plan and the Regional Comprehensive Plan and Guide.

The Southern California Air Quality Management District (SCAQMD) includes criteria for determining the significance of potential air quality impacts in its "CEQA Air Quality Handbook" (CEQA Handbook) adopted in February 1993 and amended in November 1993. The daily significance thresholds for air quality emissions from an individual project have been established by the SCAQMD for the Southern California Air Basin (So CAB). Significance thresholds for project construction and operation are shown on Table 5.1.

**Table 5.1
SIGNIFICANCE THRESHOLDS**

Pollutant	Construction Threshold (lb/day)	Operational Threshold (lb/day)
Carbon Monoxide (CO)	550	550
Sulfur Oxides (SO ₂)	150	150
Reactive Organic Gases (ROG)	75	55
Oxides of Nitrogen (NO _x)	100	55
Particulate Matter (PM ₁₀)	150	150
Particulate Matter (PM _{2.5})	55	55

Site Preparation and Construction Emissions

The proposed project includes the development of approximately 24.42 acres. Development of this project is proposed to include parking facilities for up to 764 vehicles, a baseball field and soccer field and flood control improvements.

The Air Quality Impact Analysis assumed that the construction phase of the project would occur in three phases. The first phase would be the initial excavation and hauling phase, the second would be extended hauling phase and the third would be paving and finish work. Construction would likely take place during a two year period. It is anticipated that the proposed work schedule will be approximately 20 days per month with eight hours per day. Emission estimates include fugitive dust as well as exhaust emissions. Site preparation will include grading of ground surfaces, with an export of approximately 700,000 cubic yards of fill material from the project site.

Heavy duty construction equipment would be used during grading and construction. A work crew would conduct all of the activities required to support the project site preparation and construction, the following emissions would be generated based upon the SCAQMD's "CEQA Air Quality Handbook" (CEQA Handbook).

Table 5.2 lists the air pollutant emissions based on 85 20-mile round trips per day of dirt hauling during initial excavation and hauling and 116 twenty-mile round trips per day of dirt hauling during the extended dirt hauling period. As shown in Table 5.2, air quality impacts to all emissions except Oxides of Nitrogen can be reduced to less than significant by following standard Best Available Control Measures. With the mitigation measures and by limiting the number and/or length of trips for dirt-hauling, Oxides of Nitrogen emissions will also be reduced to a less than significant level.

**Table 5.2
CONSTRUCTION ACTIVITY EMISSIONS (POUNDS/DAY)**

Activity	ROG	NOx	CO	SO ₂	PM-10 Total	PM-10 Fugitive	PM-2.5 Total	PM-2.5 Fugitive	CO ₂
Initial Excavation & Hauling									
No Mitigation	9.2	100.6	44.6	0.1	64.6	60.2	16.6	12.6	10,967.8
With Mitigation	9.2	94.0	44.6	0.1	8.4	5.8	3.6	1.2	10,967.8
Extended Hauling									
No Mitigation	8.5	104.3	41.5	0.1	4.2	60.3	16.5	12.6	12,206.4
With Mitigation	8.5	95.8	41.5	0.1	7.4	4.0	4.0	0.9	12,206.4
Paving & Finish Work									
No Mitigation	2.2	10.3	5.8	0.0	0.8	0.0	0.7	0.0	1,075.5
With mitigation	2.2	9.1	5.8	0.0	0.2	0.0	0.2	0.0	1,075.5
SCAQMD Threshold	75	100	550	150	150	-	55	-	-
Exceeding SCAQMD Thresholds	No	No	No	No	No	-	No	-	-

Source: Giroux and Associates, 2008

Table 5.3 provides hauling scenarios that will provide emissions below the thresholds of significance for air quality. It is assumed that there will be an improvement in engine emissions from 2009 to 2010 which is the reason for each year to be listed separately

**Table 5.3
ALTERNATIVE DIRT-HAULING SCENARIOS**

Analysis Year	Murrieta (5 mi. one way) trips per day	Temecula (10 mi. one way) trips per day	Perris (15 mi. one way) trips per day	Norco (25 mi. one way) trips per day
2009	245	123	82	49
2010	273	137	91	55

Operational Emissions

The operation of the proposed development will increase air emissions. Increased criteria pollutant air emissions will result from mobile operational sources such vehicles transporting people who will use the sports fields. When completed, the proposed development is projected to generate 30 additional trips during the morning peak hour and 88 additional trips during the afternoon peak hour. This amount of trips and the associated activities (which would contribute to area source missions) would not create emissions that would exceed significance thresholds (Giroux and Associates). Therefore, operational mobile emissions would not result in a permanent significant adverse impact to air quality.

Toxic Air Emissions

The parking lot expansion and two sports fields proposed do not include activities with a potential to emit measurable, significant, quantities of toxic air pollutants.

During construction, the proposed project includes operations that will have diesel odors associated with equipment and materials. None of these odors are permanent, nor are they normally considered so offensive as to cause sensitive receptors to complain. Diesel fuel odors from construction equipment and new asphalt paving fall into this category. Both based on the short-term of the emissions and the characteristics of these emissions, no significant odor impacts are forecast to result from implementing the proposed project.

Thus with mitigation incorporated, construction of the proposed project will not result in potentially significant adverse impacts to air quality. Mitigation measures for potentially significant impacts from construction are discussed below. Therefore, operation of the proposed project will result in less than significant air quality impacts and no mitigation is required for that issue area.

- d. The proposed project would not expose sensitive receptors which are located within 1 mile of the project site to project substantial point source emissions. The nearest single family residences are located 300-feet west of the proposed project site. Additionally, the Cornerstone Community Church campus contains a Kindergarten through 12 school and extended childcare facility adjacent to the proposed project area. Cornerstone School has approximately 300 students in attendance during the school year. Impacts from particulate matter and NO_x off-site may be a concern during construction. Therefore, mitigation measures are included to reduce these impacts to less than significant. The exposure would be relatively short-term and intermittent due to its occurrence only during grading and construction and would be considered more of a nuisance than a health-concern. These impacts would not occur during operation of the project. Therefore, with mitigation this is a less than significant impact.
- e. The proposed project does not involve the construction of a sensitive receptor located within one mile of an existing substantial point source emitter. A parking lot and sports fields are not considered a point source emitter or a sensitive receptor. Therefore there is no impact.
- f. The proposed project would not create a significant amount of objectionable odors affecting a substantial number of people. Odors expected to be generated by this project will be primarily those from the construction equipment and grading activities. These odors will be associated with exhaust emissions from the consumption of petroleum products (gasoline, diesel, etc.) and the paving materials used to construct the parking lot. Such odors are common in the suburban and urban areas. No substantial short-term or long-term odor impacts are anticipated to occur from project implementation. Therefore, this is a less than significant impact.

Greenhouse Gas Emissions

Implementation of the proposed project would contribute to temporary increases in greenhouse gases (GHGs) from on-road traffic increases and fuel combustion emissions from off-road construction equipment. Long-term GHG emissions from increased traffic accommodated by the additional recreation space and parking are minimal. Since California imports about 20 to 25 percent of its total electricity (mainly from the northwestern and southwestern states), GHG emissions associated with electricity

generation could also occur outside of California. Short-term GHG emissions will also derive from construction activities.

During project construction, the URBEMIS2007 computer model predicts that a peak activity day will generate the following CO₂ emissions:

Initial Excavation and Hauling	10,968 pounds/day
Extended Hauling	12,206 pounds/day
Paving and Finish Work	1,075 pounds/day

For purposes of analysis, it was assumed that non-CO₂ GHG emissions are negligible, and that the total project construction GHG burden can be characterized by 5 day work weeks for the duration of the project construction. The estimated annual GHG impact is as follows:

2009	10,968 pounds/day x 21 days	=	230,328 pounds
2009	12,206 pounds/day x 154 days	=	1,897,724 pounds
2009	Total	=	2,110,052 pounds
2010	12,206 pounds/day x 261 days	=	3,185,766 pounds
2011	1,075 pounds/day x 21 days	=	22,575 pounds

Worst-case year = 3,185,766 pounds = 1,593 tons/year

In 2004, the statewide annual GHG inventory in CO₂-equivalent levels (including all non-CO₂ gases weighted by their thermal absorption potential) was 492,000,000 metric tons (541,000,000 short tons). The worst-case project construction impact of 1,593 tons represents slightly under 0.0003% percent of the statewide burden.

There are no adopted thresholds of GHG emissions significance. However, GHG emissions are implicated in the acceleration of global warming experienced in the last several decades. Climatic impacts are global in scale. Any project-specific contribution to the global issue is miniscule. In the absence of any definitive thresholds of significance, the GHG emphasis on a project-specific level is to incorporate project design features that reduce energy consumption and reduce vehicular travel as much as is reasonably feasible. Unless there is a greater shift to clean energy such as solar, hydroelectric, wind, nuclear, etc., no substantial reduction in GHG is likely attainable by conventional methods except through energy conservation.

Mitigation: The following mitigation measures shall be implemented in order to reduce project impacts:

- 5-1 Limit the number of miles per day traveled for off-site dirt-hauling and disposal to the equivalent of 85 twenty-mile round-trips during the most intensive grading and the equivalent of 116 twenty-mile round trips during the remainder of the dirt-hauling operation.**
- 5-2 Use appropriate emission control devices on gasoline and diesel construction equipment and maintain construction equipment engines by keeping them tuned.**

- 5-3 Prohibit extended idling (more than 5 minutes) and other unnecessary operation of equipment.**
- 5-4 Utilize existing electrical power sources (i.e., temporary power poles) and avoid onsite power generation.**
- 5-5 Have sufficient equipment at the site to carry out dust-control measures in all areas covered by the contract work (not just the immediate area of construction).**
- 5-6 Employ construction activity management techniques, such as: configuring the construction parking to minimize traffic interference; extending the construction period; reducing the number of pieces of equipment used simultaneously; increasing the distance between the emission sources; and reducing or changing the hours of construction to minimize construction activity emissions.**
- 5-7 Cover loaded trucks used in construction operations with tarpaulins or maintain at least 2 feet of freeboard and wash off trucks leaving the site.**
- 5-8 Sweep streets if silt is carried over to adjacent public thoroughfares.**

The proposed project shall submit a plan to control fugitive dust using the measures outlined above and additional measures and through implementation of other reasonably available dust control measures which includes Best Available Control Measures. It shall be prepared and submitted to the City of Wildomar for approval prior to the issuance of any grading permits associated with the project. The plan shall specify the fugitive dust control measures to be employed, including the additional measures outlined below.

- 5-9 The project proponent shall comply with all applicable SCAQMD Rules and Regulations. In particular, SCAQMD Rule 403 shall be adhered to, insuring the clean-up of construction-related dirt on approach routes to the site. Rule 403 prohibits the release of fugitive dust emissions from any active operation, open storage pile, or disturbed surface area beyond the property line of the emission source. Particulate matter deposits on public roadways are also prohibited.**
- 5-10 Adequate watering techniques shall be employed to partially mitigate the impact of construction-generated dust particulates. Portions of the project site that are under-going earth moving operations shall be watered such that a crust will be formed on the ground surface and then watered again at the end of the day.**
- 5-11 Construction activities should be scheduled to occur first on the upwind portion of the project site to reduce the potential for fugitive dust impacts in the downwind areas.**
- 5-12 Any vegetative ground cover to be utilized onsite shall be planted as soon as possible to reduce the disturbed area subject to wind erosion. Irrigation systems needed to water these plants shall be installed as soon as possible to maintain the ground cover and minimize wind erosion of the soil.**
- 5-13 Any construction access roads (other than temporary access roads) shall be paved as soon as possible and cleaned after each work day. The maximum vehicle speed limit on unpaved roads shall be 15 mph.**

5-14 All material stockpiles subject to wind erosion during construction activities, that will not be utilized within three days, shall be covered with plastic, an alternative cover deemed equivalent to plastic, or sprayed with a nontoxic chemical stabilizer.

5-15 Where vehicles leave the construction site and enter adjacent public streets, the streets shall be swept daily or washed down at the end of the work day to remove soil tracked onto the paved surface.

5-16 All diesel-powered vehicles shall be turned off when not in use for more than 10 minutes and gasoline - powered equipment shall be turned off when not in use for more than five minutes.

5-17 The construction contractor shall utilize electric or natural gas powered equipment in lieu of gasoline or diesel powered engines, where feasible and where economically competitive.

Monitoring: The above described mitigation measures will be verified during grading and construction by including the requirements in the grading and construction contracts and by on-going field inspections during grading and construction. Inspection notes verifying implementation of the mitigation measures shall be retained in the project file.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
BIOLOGICAL RESOURCES - Would the project:				
6. Wildlife & Vegetation				
a) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Conservation Community Plan, or other approved local, regional, or state conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Have a substantial adverse effect, either directly or through habitat modifications, on any endangered, or threatened species, as listed in Title 14 of the California Code of Regulations (Sections 670.2 or 670.5) or in Title 50, Code of Federal Regulations (Sections 17.11 or 17.12)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
g) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, MSHCP Compliance Report, prepared by Principe and Associates, March 2007; MSHCP Compliance report and Burrowing Owl Habitat Assessment, May 2008

Findings of Fact: Vegetation growing on the site includes non-native grassland, Riversidean Sage Scrub, Coastal Sage Scrub, and areas of bare ground. According to the 2007 Principe study, there are numerous ephemeral drainages on the site. Two of these have defined channels and banks. However, these are considered isolated as they have no upstream components, originating at the top of on-site ridges and having channels and banks of only 225 and 245 feet long. There is no direct connectivity to Murrieta Creek. Therefore, on-site drainages are not “Waters of the U.S.” under the jurisdiction of the Army Corps of Engineers or “Waters of the State” under the jurisdiction of the California Department of Fish and Game.

- a. The proposed project site is located within the Western Riverside County Multiple Species Habitat Conservation Plan Area. The proposed project is not located within or near a MSHCP criteria cell area. The project is within the range of the Stephens kangaroo rat and is within the Stephens Kangaroo Rat Habitat Conservation Plan Fee Area. Paying a fee to the Riverside County Habitat Conservation Agency is considered adequate mitigation to reduce any impacts of regional urbanization to this species. California gnatcatchers were observed on the site in 2001 and 2004. Take of gnatcatchers on the site is authorized under the MSHCP. In addition, the MSHCP compliance report prepared for the project determined that the proposed project would comply with the provisions of the MSHCP. Therefore, this is less than significant impact.
- b. The proposed project would not have a substantial adverse effect, either directly or through habitat modifications, on any endangered, or threatened species, as listed in Title 14 of the California Code of Regulations (Sections 670.2 or 670.5) or in Title 50, Code of Federal Regulations (Sections 17.11 or 17.12). No endangered or threatened species were found on site during the recent site surveys. The proposed project site is not in or near a Criteria Cell, cell group, or sub-unit in the Elsinore Plan Area of the MSHCP. The site is located within the burrowing owl survey area and was surveyed for burrowing owl (*Athene cunicularia*). No habitat for the owl or evidence of its presence, now or within the recent past, was found on site or in close proximity to the site. No Burrowing owls were found on site and there is no suitable habitat for the species. No other endangered or threatened species are expected to occur on site due to regular discing of the site as part of weed control measures and its location adjacent to development. Therefore, this is less than significant impact.
- c. The proposed project would not have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Wildlife Service. No endangered or other special status species were found on site whose take would be considered significant. California gnatcatchers may be on site, but their take is authorized under the MSHCP. Therefore, this is less than significant impact.
- d. The proposed project site is adjacent to existing development and would not interfere substantially with the movement of any native resident or migratory fish or wildlife species or

with established native resident migratory wildlife corridors, or impede the use of native wildlife nursery sites. No wildlife nursery sites are located on site and the proposed project area is not considered part of a wildlife corridor or other habitat-linkage. Therefore, this is less than significant impact.

- e. There are numerous ephemeral drainages on the proposed project site’s eastern area. Two of the drainages have defined channels and banks. However, these are considered isolated as they have no upstream components, originating at the top of on-site ridges and having channels and banks of only 225 and 245 feet long. There is also no direct connectivity to Murrieta Creek. On-site drainages are not “Waters of the U.S.” under the jurisdiction of the Army Corps of Engineers or “Waters of the State” under the jurisdiction of the California Department of Fish and Game. The 2008 Principe report found that there is no riparian habitat located on the proposed project site. Therefore, the proposed project would not have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service. Therefore, this is less than significant impact.
- f. There are no wetlands located on the project site. Therefore, the proposed project would not have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means. Therefore, this is less than significant impact.
- g. The proposed project would not conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance. The site has been disced and has no trees. Therefore, this is less than significant impact.

Mitigation: The project proponent shall pay the open space related impact fees for the SKR and the MSHCP fees prior to rough grading. This standard requirement will reduce any impacts to a less than significant level.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
CULTURAL RESOURCES - Would the project:				
7. Historic Resources				
a) Alter or destroy an historic site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Cause a substantial adverse change in the significance of a historical resource as defined in California Code of Regulations, Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: General Plan, A Phase 1 Cultural Resources Assessment of Cornerstone Church Expansion, Jean Keller, November 2004 and A Phase 1 Cultural Resources Assessment of Cornerstone Church Expansion, Jean Keller, March 2009

Findings of Fact: A Phase 1 archaeological records review and survey (Study) was conducted on the proposed project site. The purpose of the Study was to obtain information pertaining to previous land uses of the subject property, and to make a determination as to what extent existing cultural resources would be impacted by the implementation of the proposed project.

- a. The proposed project would not alter or destroy an historic site. The project site has been disturbed and diked regularly for weed control. No historic structures or evidence of historic structures are found on site. Therefore, there would be no impact.
- b. The proposed project would not cause a substantial adverse change in the significance of a historical resource as defined in California Code of Regulations, Section 15064.5. No historic resources exist on the proposed project site. Therefore, there would be no impact.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
8. Archaeological Resources				
a) Alter or destroy an archaeological site.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to California Code of Regulations, Section 15064.5?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
d) Restrict existing religious or sacred uses within the potential impact area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan and a Phase 1 Cultural Resources Assessment of Cornerstone Church Expansion, Jean Keller, November 2004 and A Phase 1 Cultural Resources Assessment of Cornerstone Church Expansion, Jean Keller, March 2009

Findings of Fact: Eight cultural resource studies have been recorded within a one-half mile radius of the site. No archaeological resources were found during any of these studies. No archaeological resources were identified on the project site during the field survey. The proposed project area would not be considered sensitive for cultural resources. However, archaeological resources may be found during ground disturbing activities.

- a. Two cultural resources studies were prepared and no archaeological resources were identified through a records search and during a field survey. Although, no archaeological resources were found on site during the cultural resources survey, there may be undiscovered resources present. Therefore, if any archaeological resources are discovered during ground disturbing activities, all work would stop in the area of the find to allow for an evaluation of its significance and determination of appropriate actions, such as additional investigation and data recovery, would be made by a qualified archaeologist. Through the implementation of this mitigation, potential impacts to archaeological resources would be reduced to less than significant.
- b. Archaeological resources were not found on the project site during the records search or the field survey. The proposed project would not cause a substantial adverse change in the significance of an archaeological resource pursuant to California Code of Regulations, Section 15064.5. If any archaeological resources are discovered during these ground disturbing activities, all work would stop in the area of the find to allow for an evaluation of its significance and determination of appropriate actions, such as additional investigation and data recovery, would be made by a qualified archaeologist. Through the implementation of

this mitigation, potential impacts to archaeological resources would be reduced to less than significant.

- c. The proposed project site is not located in an area of a known formal cemetery. Therefore, it is not anticipated that human remains will be found on the project site. If any human remains are discovered during these ground disturbing activities, all work would stop in the area of the find and the Riverside County coroner's office and Pechanga Band of Luiseño Indians would be contacted to allow for an evaluation of the proper disposition of the remains. Through the implementation of this mitigation, potential impacts to human remains would be reduced to less than significant.
- d. The proposed project site is adjacent to a Christian church campus. The portion of the site that would be disturbed during project implementation has been disced for weed control purposes, but it is unlikely that any on-going pre-historic or historic religious or sacred uses have been practiced at this location. However, if evidence of such uses is discovered during ground disturbing activities, all work would stop in the area of the find to allow for an evaluation of its significance and determination of appropriate actions, such as additional investigation and data recovery, would be made by a qualified archaeologist. Through the implementation of this mitigation, potential impacts to existing religious or sacred uses would be reduced to less than significant.

Mitigation: The following mitigation measures shall be implemented to reduce potential impacts to archaeological resources to less than significant:

8-1 In the event that cultural resources, not previously identified, are encountered during project construction, construction activities will be halted or redirected until a qualified archaeologist can evaluate the nature and significance of the finds and recommend and implement management actions to protect or curate any resources that merit management.

8-2 If any human remains are encountered during initial grading activities, all ground disturbing activities in the vicinity of the discovery will be terminated immediately and the County Coroner's office must be contacted within 24 hours to arrange for management of such remains.

Monitoring: The developer shall identify the qualified archaeologist to the City, and if subsurface resources are accidentally exposed, the results and findings of the evaluation shall be provided to the City for retention in the project file.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
9. Paleontological Resources				
Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Paleontological Sensitivity Resources Map Figure OS-8 and Riverside County Land Information System.

Findings of Fact: The southern part of the proposed project site is considered in an area of high paleontological sensitivity, while the northern part of the site is considered in an area of low paleontological sensitivity. Monitoring by a qualified paleontologist full-time during ground disturbance would reduce these potential impacts to less than significant.

Mitigation: The following mitigation measures will be implemented to prevent potential impacts to paleontological resources.

9-1 A signed mitigation contract will be a condition of grading permit issuance. If paleontological resources are discovered during project construction, all work in the area of the find shall cease, and a qualified paleontologist shall be retained by the project sponsor to investigate the find, and to make recommendations on its disposition. The City shall be notified of any discoveries, and that the Planning Director shall determine the ultimate disposition of any discoveries. The developer shall implement the paleontologists recommendation.

Monitoring: The City Planning Department shall be notified if paleontological resources are accidentally unearthed on the project site. Any reports of findings shall be provided to the City and retained in the project file.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact	
GEOLOGY AND SOILS					
10. Alquist-Priolo Earthquake Fault Zone or County Fault Hazard Zones					
a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
b) Be subject to rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
APEF-Zones	NA <input type="checkbox"/>	S <input checked="" type="checkbox"/>	PS <input type="checkbox"/>	U <input type="checkbox"/>	R <input type="checkbox"/>
CFH Zones	NA <input type="checkbox"/>	S <input checked="" type="checkbox"/>	PS <input type="checkbox"/>	U <input type="checkbox"/>	R <input type="checkbox"/>
Definitions for Land Use Suitability Ratings					
Where indicated below, the appropriate Land Use Suitability Rating(s) has been checked.					
NA - Not Applicable_ S - Generally Suitable PS - Provisionally Suitable					
U - Generally Unsuitable R – Restricted					

Sources: General Plan and Geotechnical Feasibility Study, EnGen Corporation, March 2006.

Findings of Fact: According to the Geotechnical Study, the site is located in a seismically active area typical of southern California and is likely to experience ground shaking due to earthquakes on nearby faults. There are no known active or potentially active faults crossing the site. The proposed site is not within, but is near, an Alquist Priolo Study Zone. The closest Alquist Priolo Study Zone, for the Elsinore Fault Zone, is located approximately 2.2 miles from the proposed project site. The maximum credible earthquake for the Elsinore-Temecula Fault Zone is 6.8 on the Richter Scale. Since no structures are proposed as part of this project, any impacts from a seismic event are expected to be less than significant. Therefore, no mitigation measures are required.

Mitigation: No mitigation measures are required from impacts due to rupture of a known earthquake fault. Recommendations in the geotechnical evaluation will be implemented as required by the City Engineer to ensure the stability of the proposed slopes.

Monitoring: No mitigation measures are required; therefore, no monitoring will be necessary for impacts due to rupture of a known earthquake fault.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
11. Liquefaction Potential Zone Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
NA <input type="checkbox"/> S <input checked="" type="checkbox"/> PS <input type="checkbox"/> U <input type="checkbox"/> R <input type="checkbox"/>				

Sources: General Plan, and Geotechnical Feasibility Study, EnGen Corporation, March 2006.

Findings of Fact: According to the General Plan and the geotechnical investigation states that the site is not considered subject to liquefaction.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
12. Groundshaking Zone Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
NA <input type="checkbox"/> S <input checked="" type="checkbox"/> PS <input type="checkbox"/> U <input type="checkbox"/> R <input type="checkbox"/>				

Sources: General Plan and Geotechnical Feasibility Study, EnGen Corporation, March 2006.

Findings of Fact: The Elsinore fault is the nearest fault to the proposed project site and lies approximately 2.2 miles southwest of the project site. As is typical in southern California, the site may be subject to relatively strong ground motions. No inhabitable structures are proposed for the project as it consists of a parking lot and sports field project. Therefore, the proposed project shall be designed to meet the State of California standards for this seismic hazard zone to ensure that the proposed project will not result in significant impacts due to seismic ground shaking. Implementation of the recommendations of the geotechnical evaluation and Riverside County standards for grading compaction will reduce the potential for ground shaking impacts to a less than significant level.

Mitigation: The proposed project shall be designed to meet the geotechnical evaluation and State of California standards for grading compaction to ensure that the proposed project will not result in significant impacts due to seismic ground shaking, but does not require special mitigation.

Monitoring: Grading and site plans shall be reviewed and approved by the Planning Department. Field inspection during grading and construction shall verify implementation in accordance with approved plans.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact

13. Landslide Risk

Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, collapse, or rockfall hazards?

NA S PS U R

Sources: General Plan and Geotechnical Feasibility Study, EnGen Corporation, March 2006.

Findings of Fact: The project site contains mixed topography, with rolling areas and moderate slopes. Most of the steeper slopes will be retained as undisturbed open space. Other areas would be graded and compacted based on the recommendations of the geotechnical recommendation and will not be subject to landslide risk. As a result, the proposed project has no potential to expose the proposed facilities to any landslide or mudslide hazards.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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14. Ground Subsidence

Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in ground subsidence?

Sources: General Plan and Geotechnical Feasibility Study, EnGen Corporation, March 2006.

Findings of Fact: The ground subsidence (settlement) impacts have been given detailed site specific consideration in the geotechnical study for the project site. No significant subsidence is expected to occur as a result of the project. Ground subsidence impacts resulting from the proposed project would be less than significant.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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15. Other Geologic Hazards

Such as seiche, mudflow or volcanic hazard?

Sources: General Plan and Geotechnical Feasibility Study, EnGen Corporation, March 2006.

Findings of Fact: There are no other known geologic hazards that could affect the property, including a seiche, mudflow or volcanic hazard. No active, or potentially active, volcanoes are known to occur in the project area and no topography and soils conducive to mudflow risk occur on or near the site. The nearest large body of water, Lake Elsinore is located 4.3 miles northwest of the project site and is not close enough to the site to pose a risk of seiche. Therefore, no mitigation measures are required. This issue area does not require further analysis.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
16. Slopes				
a) Change topography or ground surface relief features?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Create cut or fill slopes greater than 2:1 or higher than 10 feet?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Result in grading that affects or negates subsurface sewage disposal systems?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: General Plan and Geotechnical Feasibility Study, EnGen Corporation, March 2006.

Findings of Fact:

- a. Implementation of the project will result in modifications to the existing topography and surface relief features for approximately half of the site. Though there will be major changes in the area where the parking lot and sports fields are built, the general topography visible from the surrounding areas will remain substantially the same. No significant adverse change in the site landform is anticipated to result from the proposed project. Therefore, no impact will occur
- b. Cut or fill slopes are generally 3:1 or less. However, some of the cut or fill slopes could be greater than 2:1. The geotechnical evaluation provided the general recommendations to reduce the impact of the steeper slopes to less than significant. Through the implementation of mitigation measures impacts associated with slopes greater than 2:1 are reduced to less than significant.
- c. The project will not interfere with a subsurface sewage disposal system. No subsurface sewage system exists on site and one is not proposed as part of the project. Therefore, the project development has no potential to adversely impact this issue. The existing septic system used for the church operation is not in an area that would be disturbed as a part of the proposed project. Therefore, no impact will occur.

Mitigation: The following mitigation measures will be implemented to maintain slope stability on the project site.

- 16-1** *For cut or fill slopes no taller than 30-feet and inclined at 2:1, proper slope maintenance procedures such as installation and maintenance of drainage devices and planting of slope faces to protect from erosion shall be implemented.*
- 16-2** *Surface water shall not be allowed to flow over the slopes other than incidental rainfall. No alteration of pad gradients should be allowed that will prevent pad and roof run-off from being expediently directed to approved disposal areas away from tops of slopes.*
- 16-3** *Top of slope berms shall be constructed and compacted as part of the precise grading plans shall be maintained by the property owner. The recommended drainage patterns shall be established at the time of finish grading and maintained throughout the life of the structures.*

16-4 Concentrated surface waters entering the property from off-site sources shall be collected and directed to a permanent drainage system away from the top of slopes.

16-5 The applicant shall submit landscape plans to the Planning Department, and receive approval of prior of the plans prior to the issuance of a grading permit. The Landscape Plans shall include all of the natural area re-vegetation, parking lot landscaping, and wildland fire protection components.

Monitoring: The property owner shall be responsible for maintaining and cleaning of all interceptor ditches, drainage terraces, downdrains, and any other drainage structures that have been installed to promote slope stability. Upon completion the City shall inspect the slopes to determine the recommendations of the geotechnical evaluation have been followed.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
17. Soils				
a) Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Geotechnical Feasibility Study, EnGen Corporation, March 2006, Preliminary Water Quality Management Plan, Markham Development Management Group, Inc., August 2005.

Findings of Fact:

a-b Dues to the onsite slopes, the project has the potential to create erosion impacts. However compliance with the erosion control measures associated with the grading plans and with the provisions of the Stormwater Pollution Prevention Plan, and Preliminary Water Quality Management Plan are expected to reduce any erosion-related impacts to a less than significant impact. In addition, the soil types found on the project are not considered expansive. As a result, not significant impacts are anticipated.

Mitigation: In addition to Mitigation Measures 16-1 through 16-6, the standard grading permit requirements will ensure that any potential erosional impacts will not result in a significant impact to the environment.

Monitoring: The above described requirements and mitigation measures will be verified during grading and construction by field inspections as each stage of construction takes place.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
18. Erosion				
a) Change deposition, siltation or erosion which may modify the channel of a river or stream or the bed of a lake?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Result in any increase in water erosion either on or off site?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Geotechnical Feasibility Study, EnGen Corporation, March 2006, Preliminary Water Quality Management Plan, Markham Development Management Group, Inc., August 2005.

Findings of Fact:

a-b. The proposed project will not change deposition, siltation or erosion which may modify the channel of a river or stream or the bed of a lake. The project proponent shall adhere to City grading standards, best management practices and the SWPPP are required to control potentially significant erosion hazards. Due to the drainages and slopes located on the property, a potential for erosion does exist. Implementation on Mitigation Measures 16-1 through 16-6 and 17-1 through 17-2 will ensure that the potential for significant erosion will be controlled on the project site. With the incorporation of mitigation measures, this is a less than significant impact.

Mitigation: In addition to Mitigation Measures 16-1 through 16-6 and 17-1 and 17-2 above.

18-1 Prior to the issuance of a grading permit, the applicant shall submit a revegetation plan to the Planning Department for the proposed off-site haul route. The revegetation plan should include the use of appropriate native plant species and measures to prevent erosion.

Monitoring: The City shall monitor compliance with the Mitigation Measures 16-1 through 16-6 and 17-1 through 17-2 during grading and construction and keep verification documentation in the project file as part of the public record on the project.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
19. Wind Erosion and Blowsand from project either on or off site				
Be impacted by or result in an increase in wind erosion and blowsand, either on or off site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan.

Findings of Fact: The project site is not located within a blowsand hazards area. The natural vegetation, topography, and lack of sand eliminate the potential for a blowsand hazard on the site or in the vicinity. Short-term wind erosion has been analyzed within the Air Quality section of this document and mitigation measures are proposed to reduce impacts to a less than significant level. No additional impacts are anticipated; therefore this is a less than significant impact.

Mitigation: Air quality mitigation is included in the air quality section of this environmental assessment.

Monitoring: Monitoring would occur as part of the implementation of air quality mitigation measures.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact

HAZARDS AND HAZARDOUS MATERIALS - Would the project:

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
20. Hazards and Hazardous Materials				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Impair implementation of or physically interfere with an adopted emergency response plan or an emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Source: General Plan, Envirostar Database, State of California.

Findings of Fact:

- a. Development of the proposed project site would not result in the routine transport use or disposal of toxic or hazardous materials beyond the fertilizers and pesticides associated with sports field and landscape maintenance. The transport, storage, use, and disposal of these substances would be regulated under existing hazardous waste regulations at the local, State, and federal level and any use on the project site would be required to comply with these regulations. With implementation of existing regulations, the proposed project would not create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials. This is considered a less than significant impact.
- b. According to the Envirostar Database, there are no toxic substances known to exist on the project site, so there would be little to no risk of releasing toxic or hazardous substances into the environment during grading or project construction from existing on-site hazardous materials. During grading and construction there is a risk of a spill of petroleum products. During operation of the completed project, there is potential for accidental release of hazardous substances used on-site for sports field and landscape maintenance. The property owner will follow the regulations regarding hazardous waste cleanup measures would ensure that the impacts in this issue area are less than significant. Additionally, the proposed project will adhere to the SWPPP and BMPs will ensure impacts from accidental spills will be less than significant. With mitigation, the proposed project would not create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment.
- c. The proposed project is designed with sufficient emergency access to meet Riverside County Fire Department standards, including a sufficient on-site turning radius. The proposed project would not impair implementation of or physically interfere with an adopted emergency response plan or an emergency evacuation plan. Access to the site would be from the western portion of the church site off Monte Vista Drive and Via Carnaghi Lane on the southwest. The nearest fire station is 32637 Gruwell Street in Wildomar within 1.5 miles of

the project site. An emergency response to the proposed project site from this station would be less than five minutes. Thus, the proposed project will not impair the implementation of or physically interfere with an adopted emergency response plan or an emergency evacuation plan. This is considered a less than significant impact.

- d. The proposed project would not emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school. The project site is located within one-quarter mile of an existing school. There is a K through 12 School on the church site. However, no hazardous materials would be associated with the proposed project beyond what would be associated with grading and construction activities and ongoing landscape and sports field maintenance. Therefore, the proposed project will not emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school. This is considered a less than significant impact.
- e. The proposed project is not located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would not create a significant hazard to the public or the environment. The nearest site of known hazardous waste concern is a gasoline station on Orange Street in Wildomar within approximately one mile of the project site. The site had a leaking underground fuel tank and the problem was resolved. However, based on the Envirostar database search, no impacts from on-site hazardous waste contamination concerns are anticipated for the proposed project site. This is considered a less than significant impact.

Mitigation: The following mitigation measure shall be incorporated into the proposed project:

20-1 All spills or leakage of petroleum products during construction activities shall be immediately contained; the hazardous material identified; and the contaminated site remediated in compliance with applicable state and local regulations regarding cleanup and disposal of the contaminant released. The contaminated waste shall be collected and disposed of at an appropriately licensed disposal or treatment facility.

Monitoring: The above described mitigation measure will be verified in the field should an accidental spill or leakage occur. Records of such occurrences and subsequent completion of remediation procedures shall be kept on file.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
21. Airports				
a) Result in an inconsistency with an Airport Master Plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Require review by the Airport Land Use Commission?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) For a project within the vicinity of a private airstrip, or heliport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: General Plan

Findings of Fact: The proposed project would not result in an inconsistency with an Airport Master Plan. The proposed project is not within an Airport Master Plan area and does not require review by the ALUC. The nearest airport is Skylark Airport, a small privately-owned airport in Lake Elsinore approximately two and three-quarter miles northwest of the project site. The project site is not in the airport influence area and would not result in a safety hazard for people residing or working in an area around a private airstrip or heliport facility. No structures, lighting, objects that create excessive glare, or water features that attract birds and that would interfere with airport operations are proposed by the project. No airport related impacts are forecast to occur as a result of the implementation of the proposed project.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
22. Hazardous Fire Area Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: General Plan

Findings of Fact: The project site is not located within a hazardous fire area; therefore, implementation of the project will not expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands. No impacts are anticipated; therefore, no mitigation measures are required.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact

HYDROLOGY AND WATER QUALITY - Would the project:

23. Water Quality Impacts

a) Substantially alter the existing drainage pattern of the site or area, including the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b) Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
d) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Place housing within a 100-year flood hazard area, as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Geotechnical Feasibility Study, EnGen Corporation, March 2006, Preliminary Water Quality Management Plan, Markham Development Management Group, Inc., August 2005.

Findings of Fact:

- a. The proposed project would not substantially alter the existing drainage pattern of the site or area, including the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site. The natural on-site drainages would be slightly altered as part of the project, but this would not result in substantial erosion or siltation on or off-site. Project design and implementation of the project SWPPP would prevent this impact from occurring. Through the implementation of mitigation measures, this is a less than significant impact.
- b. The proposed project would not violate any water quality standards or waste discharge requirements. The SWPPP would be implemented to ensure that surface water quality in project drainage tributaries is protected. There is no waste discharge requirement associated with the project as the project does not involve any discharge to a sewer. Parking lot runoff will be drained into parking lot landscaping features, detention basins and bioswales and desilting would occur prior to discharge to area storm drainage system. In addition to implementation of mitigation measures 16-1 through 17-2, irrigation of sports fields and landscaping will be managed with as little runoff as practicable. The SWPPP and WQMP for the proposed project will be implemented to ensure that both short- and long-term storm runoff discharges are not unacceptably degraded by sediment or other pollutants. Therefore, the proposed project will not violate any water quality standards or waste discharge requirements.
- c. The depth to groundwater is not known on the site. However, no groundwater was encountered during the fieldwork performed for the geotechnical feasibility analysis. Based on other topography and soil and bedrock configuration, it is unlikely that groundwater would be encountered during excavation of the upper portions of the site. The lower portions of the site may contain groundwater at depths that may be encountered during project implementation.

Groundwater may be encountered during the grading phase of the project within the existing drainages. However, if grading occurs during relatively dry weather, it is not likely that groundwater will be a significant concern. Grading outside the drainages will not likely encounter groundwater.

No wells are proposed to be installed on the property. Water will be supplied by the Elsinore Valley Municipal Water District that utilizes both local groundwater and surface water, and imported water supplies, to ensure adequate water is available for consumers. No adverse impacts to groundwater resources are likely to occur from implementing the proposed project, which consists of two sports fields and a parking lot with landscaping. The stormwater detention facilities would assist in enhancing groundwater recharge. The impacts in this area are less than significant.

- d. The project would not create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff.

The project will result in changes in absorption rates and the rate and amount of surface runoff on the project site. With the introduction of concrete slabs and pavement, there will be a decrease in surface permeability by impermeable surfaces. Design measures such as detention basins, shall be incorporated on the site to ensure that downstream flows will not be increased, so that no significant impacts are anticipated as a result of this project. The standard flood control fees will be charged to the project and utilized to fund storm water management facilities downstream as part of the implementation of a long-term regional system improvement plan. No potential for significant impact from the increased runoff from the site is forecast to occur. No mitigation is required.

- e-f. The project site is not located in a flood hazard zone or 100-year flood zone and no potential for exposure to significant water related hazards exists on this site. Therefore, there is no impact associated with flood hazards.

- g. The project applicant shall adhere to best management practices designed to control discharges of pollution that could cause a significant adverse impact to surface water quality. The Water Quality Management Plan and SWPPP document prepared specifically for this project define which best management practices (BMPs) will be applied to this project and their implementation will ensure that no significant erosion and sedimentation, or other water quality degrading impacts, will occur from implementing the proposed project.

The proposed project drainage runoff would be received by Murrieta Creek, and the Santa Margarita River. This project has the potential for the discharge into varying amounts of urban pollutants such as motor oil, antifreeze, gasoline, pesticides, detergents, trash, and fertilizers. The long-term best management practices to control these pollutants from the project site are identified in the WQMP. With implementation of the SWPPP and WQMP and design of the proposed project, the potential water quality impacts of the project can be reduced to a less than significant level.

Mitigation: The project comply with the requirements of the project specific Water Quality Management Plan and SWPPP. No additional hydrology mitigation measures are required.

Monitoring: Monitoring will occur as described in the project specific SWPPP.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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24. Floodplains

Degree of Suitability in 100-Year Floodplains. As indicated below, the appropriate Degree of Suitability has been checked.

NA - Not Applicable ■ U - Generally Unsuitable □ R - Restricted □

a) Substantially alter the existing drainage pattern of the site or area, including the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Changes in absorption rates or the rate and amount of surface runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam (Dam Inundation Area)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Changes in the amount of surface water in any water body?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Geotechnical Feasibility Study, EnGen Corporation, March 2006, Preliminary Water Quality Management Plan, Markham Development Management Group, Inc., August 2005.

Findings of Fact:

- a. The proposed project will not substantially alter the existing drainage pattern of the site or area, including the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site as the future surface runoff from the project site will flow through the existing system of downstream stream channels and creeks. Design measures are incorporated on the site to ensure that downstream flows will not be increased, so that no significant impacts are anticipated as a result of this project. Therefore, this is a less than significant impact.
- b. The proposed project will not create changes in absorption rates or the rate and amount of surface runoff. Design measures are incorporated on the site to ensure that downstream flows will not be increased, so that no significant impacts are anticipated as a result of this project. With the introduction of pavement, there will be a decrease in surface permeability by impermeable surfaces. However, by capturing runoff in the project landscaping areas and installation of detention basins to capture the increase in runoff, the project will not cause any significant increase in downstream flows or change in a body of water. Natural drainage channels will remain and will continue to provide areas for groundwater recharge. The sports fields will also provide percolation opportunities during storm events. Therefore, this is a less than significant impact.
- c. The proposed project is not located in an area that will expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam. Therefore, this is a less than significant impact.
- d. The onsite management of surface runoff will control the volume of surface runoff from the site to that which already occurs. Thus, aside from detention onsite, no change in the amount of surface water in downstream channels will result from project implementation. Therefore, this is a less than significant impact.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
LAND USE/PLANNING - Would the project:				
25. Land Use				
a) Result in a substantial alteration of the present or planned land use of an area?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Affect land use within a city sphere of influence and/or within adjacent city or county boundaries?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Riverside County Land Information System.

Findings of Fact:

- a. The proposed project would not result in a substantial alteration of the present or planned land use of an area. The proposed project consists of construction of sports fields and a parking lot as part of the improvement of facilities at an existing church. Adjacent land uses are designated for rural residential and commercial uses. The combination of land uses proposed is consistent with existing and proposed levels of land use in the area. The project does not represent a change from the existing designation for this property. Therefore, this is a less than significant impact.
- b. The project is located within the City of Wildomar. The development of this project is not projected to create any incompatibilities with the surrounding uses. Development within the project area includes residences, commercial property and a church. The project is compatible with the mixed-use development in the surrounding area.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
26. Planning				
a) Be consistent with the site's existing or proposed zoning?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Be compatible with existing surrounding zoning?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Be compatible with existing and planned surrounding land uses?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Be consistent with the land use designations and policies of the Comprehensive General Plan (including those of any applicable Specific Plan)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Disrupt or divide the physical arrangement of an established community (including a low-income or minority community)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: General Plan, Riverside County Land Information System.

Findings of Fact:

- a. The proposed project is consistent with the site's existing zoning and uses. The proposed project site is designated BP (Business Park) and MDR (Medium Density Residential) and zoned as rural residential. There is a PUP on the site that allows for the development and operation of a church and associated facilities which is consistent with the zoning, land use designation and surrounding uses. The proposed project is the modification of the PUP to include a bigger parking lot and relocation of sports fields. Both of these uses are compatible with the land use entitlement being sought and are a logical extension of the existing uses at the site. There are already parking areas and a sports field being used at the church site. The proposed project would not change the uses at the site, but would offer the same types of uses and provide more paved parking area. Therefore, this is a less than significant impact.
- b. The proposed project is compatible with existing surrounding zoning. The proposed project is part of a PUP for church campus development. This mixed use area is in transition rural residential uses to higher density development including commercial uses. Surrounding zoning includes rural and medium density residential areas and commercial zones. Sports field uses already exist on site and would be considered compatible with the surrounding uses. The proposed project would be reviewed by staff to ensure compatibility through compliance with all zoning standards including landscaping and parking lot design. Therefore, this is a less than significant impact.
- c. The proposed project is compatible with existing surrounding land uses. The proposed project is part of a PUP for church campus development. This mixed use area is in transition rural residential uses to higher density development including commercial uses. Surrounding zoning includes rural and medium density residential areas and commercial zones. Sports field uses already exist on site and would be considered compatible with the surrounding uses. The proposed project would be reviewed by staff to ensure compatibility through compliance with all zoning standards including landscaping and parking lot design. Therefore, this is a less than significant impact.
- d. The proposed project is consistent with the land use designations and policies of the Riverside County Comprehensive General Plan, which the City adopted July 1, 2008. The project would comply with all policies in the plan and development code including those regarding landscaping requirements. Therefore, this is a less than significant impact.
- e. The proposed project would not disrupt or divide the physical arrangement of an established community (including a low-income or minority community). It is located on an undeveloped portion of an existing church campus. Surrounding uses include commercial and rural residential. Improvement of the church campus to provide more paved parking lot area and relocation of sports fields would be compatible with surrounding land uses. By providing more parking on-site, the proposed project would help eliminate the parking of cars on surrounding streets during well-attended church events. No impacts are foreseen from implementation of the project.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
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MINERAL RESOURCES - Would the project:

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
27. Mineral Resources				
a) Result in the loss of availability of a known mineral resource in an area classified or designated by the State that would be of value to the region or the residents of the State?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Be an incompatible land use located adjacent to a State classified or designated area or existing surface mine?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Expose people or property to hazards from proposed, existing or abandoned quarries or mines?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: General Plan, Figure OS-5

Findings of Fact: According to the General Plan Figure OS-5, the project site is in Mineral Resource Zone 3. Mineral deposits are likely to exist but the significance is undetermined. No mineral resources were specifically identified on the project site and there is no historical use of the site for mineral extraction purposes. Development of the site for mineral extraction would be very unlikely given surrounding uses. No mineral resource impacts were identified and no mitigation measures were required.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
NOISE - Would the project result in:				
Definitions for Noise Acceptability Ratings				
Where indicated below, the appropriate Noise Acceptability Rating(s) has been checked.				
NA - Not Applicable A - Generally Acceptable B - Conditionally Acceptable C - Generally Unacceptable D - Land Use Discouraged				
28. Airport Noise				
a) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
NA <input checked="" type="checkbox"/> A <input type="checkbox"/> B <input type="checkbox"/> C <input type="checkbox"/> D <input type="checkbox"/>				
b) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
NA <input checked="" type="checkbox"/> A <input type="checkbox"/> B <input type="checkbox"/> C <input type="checkbox"/> D <input type="checkbox"/>				

Sources: General Plan, Elsinore Area Plan

Findings of Fact: The project site is not located within an airport land use plan. The nearest airport is the Skylark Airport located approximately two and one-quarter miles northwest of the project site. Therefore, the project would not expose people residing or working in the project area to excessive noise levels from a public use airport. However, the proposed project site is not within the airport influence area and noise impacts associated with the airport are forecast to be less than significant at the project site.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
29. Railroad Noise	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
NA <input checked="" type="checkbox"/> A <input type="checkbox"/> B <input type="checkbox"/> C <input type="checkbox"/> D <input type="checkbox"/>				

Sources: General Plan, Figure S-21

Findings of Fact: The project site is not located near any railroad lines. No impacts will occur.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
30. Highway Noise	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
NA <input type="checkbox"/> A <input checked="" type="checkbox"/> B <input type="checkbox"/> C <input type="checkbox"/> D <input type="checkbox"/>				

Sources: General Plan, Figure S-20, Technical Appendix I - Figure 1

Findings of Fact: The west side of the project site is located within 600 to 1000 feet of Interstate 15, However, the uses of the proposed project, parking lot and sports fields, are not considered noise sensitive uses. Sports fields are compatible with ambient noise levels of 70 to 75 decibels (Noise Element). The noise level forecast for a 10 lane freeway 382 feet from centerline at General plan build-out is approximately 70 dBA and 815 feet from Centerline is approximately 65 dBA. These levels would not be exceeded at the project site and therefore the highway noise would not be considered a major source of noise. Therefore, this is considered a less than significant impact

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
31. Other Noise				
NA <input checked="" type="checkbox"/> A <input type="checkbox"/> B <input type="checkbox"/> C <input type="checkbox"/> D <input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan Noise Element

Findings of Fact: No other noise impacts have been identified.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
32. Noise Effects on or by the Project				
a) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan

Findings of Fact: In the Noise Element of the General Plan, a noise exposure up to 70 or 75 dBA CNEL is considered acceptable for sports fields and there is no noise standard for a parking lot. Noise levels at the proposed project are not expected to exceed this level.

Existing noise levels throughout the project area derive almost exclusively from vehicular sources on the highways and secondary roads in the area. Roadway traffic noise will increase in direct proportion to the number of vehicles on area roadways. The noise level forecast for a 10 lane freeway 382 feet from centerline at General plan build-out is approximately 70 dBA and 815 feet from Centerline is approximately 65 dBA. These levels would not be exceeded at the project site and therefore the highway noise would not be considered a major source of noise, even at general plan build-out.

No formal noise study has been prepared for the proposed project. Implementing the proposed development will generate noise during construction and following occupancy of the site. The construction noise is controlled by County requirements that construction activities be restricted to the hours of 6:00 am to 6:00 pm June through September and 7:00 am to 6:00 pm October through May. In addition, construction activities may result in the exposure of grading and construction employees to severe noise levels are generally considered to be sounds greater than 75 dBA for several hours. Occupational Safety and Health Administration (OSHA) requires hearing protection for persons exposed to 75 dBA for more than eight hours per day or exposed to extreme (90+ dBA) impulse sounds. Construction contractors must comply with OSHA hearing protection requirements by establishing a program which will include a hearing protection program for those operations that

exceed hearing protection thresholds. Proposed modified project operations will be included in this hearing protection program and, therefore, implementation of the project is not forecast to expose people to severe noise levels without protection. No additional mitigation is required.

The impacts of grading and dirt removal operations on nearby sensitive receptors, specifically the low density residential uses adjacent to the project and the school use on the adjoining church campus, would be considered nuisance noise as the duration of the project grading and construction. There may also be a generation of groundborne vibration or groundborne noise from grading equipment and earthwork. Mitigation measures are recommended to ensure these impacts are reduced to less than significant.

The ongoing noise levels associated with the additional sports field uses are considered acceptable and less than significant for the project area in accordance with General Plan Noise Element policies. Specifically, sports field uses will increase onsite noise relative to existing noise generation during games and practice, but the ongoing noise levels will be consistent with the current noise environment. The background noise levels in the area are dominated by the I-15 freeway corridor (75 dB CNEL adjacent to the freeway and 70 dBA at the western edge of the site). The outdoor sports activities associated with the proposed project are not forecast to generate noise levels that exceed the existing background noise level.

Mitigation: Implementation of the following grading and construction noise mitigation measures can reduce potential noise impacts to a level of non-significance.

- 32-1 Grading, earth removal, and construction shall be limited to daylight hours and during the hours of 7 a.m. to 6 p.m. on Monday through Friday, and 9 a.m. to 6 p.m. on Saturday, and shall be prohibited on Sundays and federal holidays, except in emergencies.**
- 32-2 Utilize construction methods or equipment that will provide the lowest level of noise impact, i.e., use newer equipment that will generate lower noise levels.**
- 32-3 All construction vehicles and fixed or mobile equipment shall be equipped with properly operating and maintained mufflers or sound attenuation devices, as specified in regulations at the time of construction.**
- 32-4 Schedule the construction such that the absolute minimum number of equipment would be operating at the same time.**
- 32-5 All employees that will be exposed to noise levels greater than 75 dB over an 8-hour period shall be provided with adequate hearing protection devices to ensure no hearing damage will result from construction activities.**
- 32-6 If equipment is being used that can cause hearing damage at adjacent noise receptor locations (distance attenuation shall be taken into account), portable noise barriers shall be installed that are demonstrated to be adequate to reduce noise levels at receptor locations below hearing damage thresholds. This may include erection of temporary berms or plywood barriers to create a break in the line-of-sight, or erection of a heavy fabric tent around the noise source.**
- 32-7 The City shall require that a noise control plan be submitted for all impulsive construction equipment such as pile drivers, jackhammers, rockcrushers, etc, and all mobile equipment over 200 HP proposed to operate within 160 feet of an**

occupied residence as part of the grading permit application. The noise control plan shall outline measures to be taken to minimize noise nuisance and must be approved by the City prior to use of the impulsive construction equipment. Use of impulsive construction equipment such as pile drivers, jackhammers, rockcrushers, etc. will be limited during the regular school hours of the school buildings located within 1000 ft. of the noise generator.

Monitoring: The above described mitigation measure will be verified in the field during construction by City of Wildomar field inspections. Records of field observations and subsequent remediation procedures shall be kept on file.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
POPULATION AND HOUSING Would the project				
33. Housing				
a) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create a demand for additional housing, particularly housing affordable to households earning 80% or less of the County's median income?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Affect a County Redevelopment Project Area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Cumulatively exceed official regional or local population projections?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan

Findings of Fact:

- a. Implementation of the project will not displace any existing housing as the site is currently vacant and will, therefore, not necessitate the construction of replacement housing elsewhere. Therefore, no impact will occur.
- b. The project will not create any significant demand for housing. The proposed project is part of an existing church campus that primarily serves residents in the nearby area. Development of additional parking and sports fields would have no impact on housing supply or demand. Therefore, no impact will occur.
- c. The proposed project would not displace substantial numbers of people, necessitating the construction of replacement housing elsewhere. There are no homes on the project site and there is nothing in the character, design, or implementation of the proposed project that would displace people in the homes adjacent to the project. Therefore, no impact will occur.
- d. The proposed project is not in or near a County redevelopment area. Therefore, no impact will occur.

- e. The proposed project is not residential in nature and would therefore not contribute to population within the project area. Therefore, no impact will occur.
- f. The proposed project would not be considered growth-inducing. It is the modification of an existing church campus to include additional parking and relocate sports fields. All required infrastructure is available either adjacent to or near the project site. Therefore, no major extension of infrastructure, and related growth inducement, will result from implementing the proposed project. No significant population or housing impacts are forecast to occur from project implementation. Therefore, no impact will occur.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

PUBLIC SERVICES Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered government facilities or the need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
34. Fire Services	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Riverside County Fire Department website.

Findings of Fact: The project site is served by the Riverside County Fire Department. The closest station to the project site is the Wildomar Station, #61, located at 32637 Gruwell Street. This station is within 1.5 miles of the project site.

Implementation of the proposed project will result in a less than significant impact to fire services. The project may incrementally add to the existing demand for fire services. The relocated sports fields would generate virtually no impact and a parking lot would generate no demand for fire services. Operational expenses are covered by the County's General Fund and the project contributes property taxes to the general fund to offset the potential incremental demand for fire protection services.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
35. Sheriff Services	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan

Findings of Fact: The proposed project would have law enforcement services available from the Riverside County Sheriff's Department and the California Highway Patrol. The Sheriff's Department

serves the community of Wildomar, with the Lake Elsinore station at 333 Limited Avenue providing service to the proposed project. The California Highway Patrol has jurisdiction along Interstate 15.

Implementation of the proposed project will result in a less than significant impact to sheriff services. The project may incrementally add to the existing demand for sheriff services and/or the need for new facilities. The project will contribute property taxes to the general fund to offset the potential incremental demand for police protection services.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
36. Schools	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: General Plan

Findings of Fact: The proposed project is located within the Elsinore Valley Unified School District, but is not near any existing or proposed public school site. The proposed project does not provide housing and will not create a demand for school services. The existing church facility provides alternate education from grades K through 12. The proposed project will not effect the current education program at the facility. Implementation of the proposed project will result in no impact to public schools. The proposed sports fields will likely be used by the school located on the church campus. This would be considered a benefit to the existing school.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
37. Libraries	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: General Plan

Findings of Fact: Implementation of the project will result in no impact to library services. The project is forecast to cause no increase in the local population. As a result, the project will not add to the existing demand on libraries and does not require additional library services.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
38. Health Services	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan

Findings of Fact: Implementation of the project will result in a less than significant impact to health services. Health care service is provided by the private sector, and health care capacity expands in response to additional demand. The proposed commercial development will result in less than significant incremental demand for healthcare services.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
RECREATION				
39. Parks and Recreation				
a) Would the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Would the project include the use of existing neighborhood or regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Is the project located within a C.S.A. or recreation and park district with a Community Parks and Recreation Plan (Quimby fees)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: General Plan and Site Plan.

Findings of Fact: The proposed project is the expansion of a church campus to include additional parking and relocate sports fields for recreation. It provides, but would not place direct demand on recreational facilities. It may serve to incrementally decrease demand on public recreational facilities as the private facilities are used in lieu of public facilities by church members. No significant impacts to parks and recreation will occur as a result of the implementation of the proposed project. The proposed project would not increase the population in the area and is not forecast to cause a significant increase in the demand for use of offsite existing neighborhood or regional parks or other recreational facilities.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
40. Recreational Trails.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: General Plan, Elsinore Area Plan

Findings of Fact: A community and public trail runs south and east of the proposed project site. However, the proposed project does not include recreational trails and would not impact the trail. No impacts to regional recreational trails will occur as a result of project implementation.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
TRANSPORTATION/TRAFFIC Would the project:				
41. Circulation				
a) Cause an increase in traffic which is substantial in relation to the existing traffic load and capacity of the street system (i.e., result in a substantial increase in either the number of vehicle trips, the volume to capacity ratio on roads, or congestion at intersections)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Result in inadequate parking capacity?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Exceed, either individually or cumulatively, a level of service standard established by the county congestion management agency for designated road or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Alter waterborne, rail or air traffic?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Substantially increase hazards to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g. farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g) Cause an effect upon, or a need for new or altered maintenance of roads?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
h) Cause an effect upon circulation during the project's construction?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
i) Result in inadequate emergency access or access to nearby uses?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
j) Conflict with adopted policies supporting alternative transportation (e.g. bus turnouts, bicycle racks)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: Site Plan; Air Quality Impact Analysis, Cornerstone Church Expansion, Giroux and Associates, May 27, 2008; Existing traffic Conditions, Urban Crossroads, May 27, 2009.

Findings of Fact: The Urban Crossroads report determined that existing conditions of the project roadways are operating at acceptable levels of conditions:

1. I-15 Southbound Ramp (S) at Bundy Canyon Drive (EW)
2. I-15 Northbound Ramp (N) at Bundy Canyon Drive (EW)
3. Monte Vista Drive (NS) at Bundy Canyon Drive (EW)
4. I-15 Southbound Ramp (S) at Baxter Road (EW)
5. I-15 Northbound Ramp (N) at Baxter Road (EW)
6. Monte Vista Drive (NS) at Baxter Road (EW)

Existing peak hour traffic operations have been evaluated and are summarized in Table 41-1 which is located on Page 44 of this Initial Study. For existing traffic conditions, the study area intersections are currently operating at an acceptable level of service (LOS "D" or better) during the weekday and weekend peak hours with existing geometry, except for the intersections of:

I-15 SB Ramps/Baxter Road (LOS “E” during AM weekday peak hour)

I-15 NB Ramps/Baxter Road (LOS “F” during AM and PM weekday peak hour)

While the existing church/school facility generates vehicle trips throughout the week, including the AM and PM peak hours, the primary impact of the church facility is on Sunday morning and early afternoon when area intersections operate at acceptable levels of service.

**Table 41.1
Intersection Analysis for Existing Conditions**

Intersection	Traffic Control ¹	Intersection Approach Lanes ²				Weekday		Saturday	Sunday
		N/B	S/B	E/B	W/B	AM	PM		
		L T R	L T R	L T R	L T R				
I-15 SB Ramp (NS) at: Bundy Canyon Rd (EW) Baxter Rd (EW)	TS	0 0 0	1 1 0	0 2 0	1 2 0	C	C	B	B
	CSS	0 0 0	0 1 1	0 1 0	1 1 0	E	D	C	D
I-15 NB Ramp (NS) at: Bundy Canyon Rd (EW) Baxter Rd (EW)	TS	1 1 0	0 0 0	1 2 0	0 2 0	C	C	B	B
	CSS	1 0 1	0 0 0	1 1 0	0 1 0	F	F	C	D
Monte Vista Rd (NS) Bundy Canyon Rd (EW) Baxter Rd (EW)	CSS	0 1 0	0 0 0	0 1 0	0 1 0	C	C	C	C
	CSS	0 0 0	0 1 0	0 1 0	0 1 0	B	A	A	B

a & c. During construction of the proposed project site, the proposed project would require up to 116 round trips per day to export dirt as well as plus some trips associated with the grading construction supplies and employees. The hauling of the exported material will be spread out throughout the work day and would have a less than significant impact on local roads and intersections at peak hour trips. Prior to the initiation of any hauling activities the applicant will need to obtain City approval of a haul permit. The haul permit contained in Mitigation Measure 41-1 will be conditioned to ensure that any impacts to the local road network will be minimized. Following project construction, the project improvements do have some potential to further exacerbate the existing traffic congestion issues created when the previous church expansion when it was approved by the County of Riverside. The City may determine as a

¹ CSS = Cross Street Stop
AWS = All Way Stop
TS = Traffic Signal

² When a right turn is designated, the lane can either be striped or un-striped. To function as a right turn lane there must be sufficient width for right turning vehicles to travel outside the through lanes.

L = Left Turn; T = Through, R = Right; 1! -= Shared Left-Through-Right; 0.5 = Shared Lane

² Delay and level of service calculated using the following software: Traffic, Version 7.9 R# (200&). Per the 2000 Highway Capacity Manual, overall average intersection delay and level of service are shown for intersections with traffic signal or all way stop control. For intersections with cross street stop control, the delay and level of service for worst individual movement (or movements sharing a single lane) are shown.

² _ = Volume to Capacity Ratio > , Level of Service “F”

condition of approval the proposed project pay a fair share toward the future traffic signal improvements in the area. Consequently, the operational impact of the project is expected to remain less than significant.

- b. The proposed project is not forecast to result in inadequate parking capacity. A major component of the project is a parking lot expansion that would provide approximately 764 additional parking spaces. This would be enough spaces to provide parking for attendees and participants at church activities. This parking will be incorporated into the existing church campus and include landscaping elements. Therefore, this is a less than significant impact.
- d-e. The proposed project is not forecast to result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks or alter waterborne, rail or air traffic. The proposed project is within two miles of Skylark Airport, but not within the influence area. The project would not result in an airspace obstruction or other condition that would change any airport operations. The project site is not near a rail line or a body of water with waterborne traffic. Therefore, this is a less than significant impact.
- f. The proposed project would not substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g. farm equipment). The proposed project has been designed with an internal parking lot circulation system that meets all City standards. There are no sharp curves or dangerous intersections in the project design. No roadways will be adversely affected by the proposed project. Improvements to local roadways, such as the paving of Via Carnaghi Lane, will be implemented in accordance with City requirements. Therefore, this is a less than significant impact.
- g. The proposed project would not cause a significant effect upon, or a significant need for new or altered maintenance of roads. The proposed project would result in improvements, such as the paving of Via Carnaghi Lane in accordance with City requirements. Access to the site would remain the same as under existing conditions. Maintenance of the on-site improvements would be performed by the project proponent. Maintenance of the off-site improvements would be performed by the City. Therefore, this is a less than significant impact.
- h. The proposed project would not result in inadequate emergency access or access to nearby uses. The project design and roadway system would meet all County standards. The two existing access points off Monte Vista Drive; one access through the church parking lot to the west parking lot and one from the improved Via Carnaghi Lane will remain unchanged. Therefore, this is a less than significant impact.
- i. The proposed project will result in temporary impacts to circulation during construction activities. During construction activities, the traffic flow will be maintained to the highest level possible with the use of standard traffic control devices. Typical traffic control measures include warning signs, warning lights, and flaggers. Implementation of standard traffic control measures, through a traffic management plan approved by the County, will provide guidance and navigational tools throughout the project area in order to maintain traffic flow and levels of safety during construction.
- j. The proposed project would not conflict with adopted policies supporting alternative transportation (e.g. bus turnouts, bicycle racks). There is no bus turnout associated with the proposed project. However, a bicycle rack could be incorporated in the parking lot design if needed.

Mitigation: The following traffic and circulation system mitigation measures will be implemented by the proposed project:

41-1 Prior to the initiation of any grading activities, the Planning Director and City Engineer shall determine if additional environmental analysis is required for the proposed haul component. No haul permit shall be issued with the prior approval of the Planning Director. The applicant shall obtain a haul route permit with a traffic control plan from the City Engineer. During construction activities, the traffic flow will be maintained to the highest level possible with the use of standard traffic control devices. Typical traffic control measures include warning signs, warning lights, and flaggers. Implementation of traffic control measures will provide guidance and navigational tools throughout the project area in order to maintain traffic flow and levels of safety during construction. At the City Engineer’s sole discretion, additional mitigation measure or restrictions may be applied to the project to minimize impacts to the surrounding community. Additional environmental analysis will be required if the potential impacts exceed the impacts identified in this Initial Study.

Monitoring: Mitigation Monitoring will be accomplished by City of Wildomar.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
42. Bike Trails	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, Elsinore Area Plan Figure 8.

Findings of Fact: The proposed project is near a designated community trail (a part of Baxter Road south of the project and continuing east of the project). This trail can be used for bicycles, but is a multi-use trail rather than a designated bike trail for bikes only. No conflicts with the use of the community trail would occur as a result of the proposed project.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
UTILITY AND SERVICE SYSTEMS - Would the project:				
43. Water				
a) Require or result in the construction of new water treatment facilities or expansion of existing facilities, the construction of which would cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan

Findings of Fact: The proposed project is located within the boundaries of the Elsinore Valley Municipal Water District (EVMWD), which would serve the proposed project with water. EVMWD utilizes both local groundwater and surface water, and imported water supplies, to ensure adequate water is available for consumers.

a-b. The proposed project has a water demand created by the additional parking lot landscaping. Existing sports fields, currently being irrigated, would be relocated as part of the proposed project. Every reasonable measure shall be taken to design and operate the irrigation systems for the proposed project to use water efficiently. No recycled water is available in the project area for grading or facilities operations. The water demand from the proposed project would be approximately 32 acre feet per year (based on the irrigation budget of 1,391,893 cubic feet of water per year total project demand). This is equivalent to the water demand of approximately 64 single family houses and considered only a small percentage of the current capacity. As such, the water demand would be considered less than significant. Elsinore Valley Municipal Water District's Urban Water Management Plan indicates that there are enough water treatment facilities to treat water needed for existing and anticipated development within its service area. The proposed project water demand would not require or result in the construction of new water treatment facilities or expansion of existing facilities since the existing church is already served by EVMWD. Any impacts will be less than significant.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
44. Sewer				
a) Require or result in the construction of new wastewater treatment facilities, including septic systems, or expansion of existing facilities, the construction of which would cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in a determination by the wastewater treatment provider which serves or may service the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: General Plan, EVMWD.org

Findings of Fact:

a-b. The proposed project is located within the boundaries of the EVMWD. However, the proposed project would not generate any demand for sewage treatment as there are no new toilets, bathing, or kitchen facilities, proposed and the church facility uses a septic system and is not connected to the sewer. Therefore, there would be no impacts in the area of wastewater treatment.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
45. Solid Waste				
a) Is the project served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Comply with federal, state, and local statutes and regulations related to solid wastes (including the CIWMP (County Integrated Waste Management Plan)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan

Findings of Fact:

a- b. The proposed project would generate solid waste during the grading and construction phase and also during operation of the facilities. The amount of solid waste from grading and construction of the parking lot and sports fields would be relatively insignificant. As a result, the amount of solid waste from operation of the facilities would be minimal (the landfills serving the project area are the Lambs Canyon and the El Sobrante landfills which currently have capacity for the foreseeable future). As a result, it is anticipated that any impacts from solid waste resources.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
46. Utilities				
Would the project impact the following facilities requiring or resulting in the construction of new facilities or the expansion of existing facilities; the construction of which could cause significant environmental effects?				
a) Electricity?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Natural gas?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Communications systems?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Storm water drainage?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Street lighting?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Maintenance of public facilities, including roads?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
g) Other governmental services?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
h) Conflict with adopted energy conservation plans?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Sources: General Plan, project site plan.

Findings of Fact: The proposed project is the modification of an existing PUP to expand the parking facilities at an existing church/school campus (including the relocation of existing sports fields. Onsite utilities improvements for storm water drainage, detention, and release would be provided. These facilities would be connected to existing infrastructure. The project stormwater drainage system would connect to the regional system. The impacts in this area are considered less than significant based on the availability of existing public facilities that support local systems. The project will not conflict with adopted energy conservation plans. Grading and construction activities would be coordinated to ensure minimal disturbance of existing utilities during project implementation. Compliance with the requirements of Southern California Edison, Southern California Gas, other utilities and Riverside County Flood Control will ensure impacts to utilities remain below a level of significance.

Mitigation: No mitigation measures are required.

Monitoring: No monitoring is required.

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
OTHER				
47. Other: No other impacts have been identified.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Sources: N/A

Findings of Fact: N/A

Mitigation: N/A

Monitoring: N/A

	Potentially Significant Impact	Less than Significant with Mitigation Incorporated	Less Than Significant Impact	No Impact
MANDATORY FINDINGS OF SIGNIFICANCE				
48. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self- sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare, or endangered plant or animal to eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
49. Does the project have the potential to achieve short-term environmental goals, to the disadvantage of long-term environmental goals? (A short-term impact on the environment is one which occurs in a relatively brief, definitive period of time while long-term impacts will endure well into the future.)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
50. Does the project have impacts which are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of an individual project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects as defined in California Code of Regulations, Section 15130)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
51. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Sources: Preceding checklist

Findings of Fact: The proposed project is the revision of an existing Public Use Permit (P.U.P.) in order to expand an existing parking lot and relocate ball fields at an existing church site. The proposed project is located on approximately 24.42 acres and would result in the construction of additional parking spaces, a baseball diamond, soccer field, and drainage improvements on an 83.01-acre church campus. Drainage facilities would include vegetated enhanced bioswales for stormwater quality improvement. In order to prepare the site for construction, up to 700,000 cubic yards of material would need to be exported from the site. When completed, the proposed development is projected to generate 30 additional trips during the morning peak hour and 88 additional trips during the afternoon peak hour. During construction, there would be up to 116 trips per day to export dirt from the site.

48. The project does not have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self- sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare, or endangered plant or animal, or to eliminate important examples of the major periods of California history or prehistory. Please refer specifically to Biological, Cultural Resource, and other sections of this initial study. With mitigation, this is a less than significant impact.

49. The project would not achieve short-term environmental goals, to the disadvantage of long-term environmental goals. There are some short-term impacts to the environment from project grading and construction that would be less than significant with mitigation, but would be considered a nuisance to surrounding properties. Please refer specifically to Air Quality, Noise, Transportation/Traffic, and other sections of this initial study. However, all long-term impacts are less than significant, or less than significant with mitigation incorporated, and would not interfere with the achievement of long-term environmental goals in the project area or at the project site.
50. The project does not have impacts which are individually limited, but cumulatively considerable and does not have environmental effects which will cause substantial adverse effects on human beings that cannot be mitigated to a less than significant level. Please refer specifically to Air Quality, Noise, Transportation/Traffic, and other sections of this initial study.
51. The project does not have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly. Aesthetics impacts, air quality and transportation/traffic impacts were all determined to be less than significant or less than significant with mitigation incorporated. All impacts were found to be less than significant or less than significant with mitigation incorporated. Mitigation measures have been identified to reduce all potentially significant impacts to less than significant. Therefore, no further analysis is required.

SUMMARY OF MITIGATION MEASURES

Aesthetics

- 3-1 The plans for the athletic field lighting and parking area security lighting shall be reviewed and approved by the Planning and Building and Safety Department prior to approval of the building permit.
- 3-2 Prior to final inspection, the outdoor lighting shall be inspected by the Building and Safety and Planning Departments to insure compliance with the approved lighting plan and ensure that potential off-site light spillage is minimized. An additional pre-field use inspection may also be required by the Planning Director. Additional lighting adjustments may be required at this time.
- 3-3 Landscaping shall be installed per the approved plans and reviewed by the Planning Department, prior to final inspection.

Air Quality

- 5-1 Limit the number of miles per day traveled for off-site dirt-hauling and disposal to the equivalent of 85 twenty-mile round-trips during the most intensive grading and the equivalent of 116 twenty-mile round trips during the remainder of the dirt-hauling operation.
- 5-2 Use appropriate emission control devices on gasoline and diesel construction equipment and maintain construction equipment engines by keeping them tuned.
- 5-3 Prohibit extended idling (more than 5 minutes) and other unnecessary operation of equipment.
- 5-4 Utilize existing electrical power sources (i.e., temporary power poles) and avoid onsite power generation.
- 5-5 Have sufficient equipment at the site to carry out dust-control measures in all areas covered by the contract work (not just the immediate area of construction).
- 5-6 Employ construction activity management techniques, such as: configuring the construction parking to minimize traffic interference; extending the construction period; reducing the number of pieces of equipment used simultaneously; increasing the distance between the emission sources; and reducing or changing the hours of construction to minimize construction activity emissions.
- 5-7 Cover loaded trucks used in construction operations with tarpaulins or maintain at least 2 feet of freeboard and wash off trucks leaving the site.
- 5-8 Sweep streets if silt is carried over to adjacent public thoroughfares.
- 5-9 The project proponent shall comply with all applicable SCAQMD Rules and Regulations. In particular, SCAQMD Rule 403 shall be adhered to, insuring the clean-up of construction-related dirt on approach routes to the site. Rule 403 prohibits the release of fugitive dust emissions from any active operation, open storage pile, or disturbed

surface area beyond the property line of the emission source. Particulate matter deposits on public roadways are also prohibited.

- 5-10 Adequate watering techniques shall be employed to partially mitigate the impact of construction-generated dust particulates. Portions of the project site that are undergoing earth moving operations shall be watered such that a crust will be formed on the ground surface and then watered again at the end of the day.
- 5-11 Construction activities should be scheduled to occur first on the upwind portion of the project site to reduce the potential for fugitive dust impacts in the downwind areas.
- 5-12 Any vegetative ground cover to be utilized onsite shall be planted as soon as possible to reduce the disturbed area subject to wind erosion. Irrigation systems needed to water these plants shall be installed as soon as possible to maintain the ground cover and minimize wind erosion of the soil.
- 5-13 Any construction access roads (other than temporary access roads) shall be paved as soon as possible and cleaned after each work day. The maximum vehicle speed limit on unpaved roads shall be 15 mph.
- 5-14 All material stockpiles subject to wind erosion during construction activities, that will not be utilized within three days, shall be covered with plastic, an alternative cover deemed equivalent to plastic, or sprayed with a nontoxic chemical stabilizer.
- 5-15 Where vehicles leave the construction site and enter adjacent public streets, the streets shall be swept daily or washed down at the end of the work day to remove soil tracked onto the paved surface.
- 5-16 All diesel-powered vehicles shall be turned off when not in use for more than 10 minutes and gasoline - powered equipment shall be turned off when not in use for more than five minutes.
- 5-17 The construction contractor shall utilize electric or natural gas powered equipment in lieu of gasoline or diesel powered engines, where feasible and where economically competitive.

Cultural Resources

- 8-1 In the event that cultural resources, not previously identified, are encountered during project construction, construction activities will be halted or redirected until a qualified archaeologist can evaluate the nature and significance of the finds and recommend and implement management actions to protect or curate any resources that merit management.
- 8-2 If any human remains are encountered during initial grading activities, all ground disturbing activities in the vicinity of the discovery will be terminated immediately and the County Coroner's office must be contacted within 24 hours to arrange for management of such remains.

Paleontological Resources

- 9-1 A signed mitigation contract will be a condition of grading permit issuance. If paleontological resources are discovered during project construction, all work in the area of the find shall cease, and a qualified paleontologist shall be retained by the project sponsor to investigate the find, and to make recommendations on its disposition. The City shall be notified of any discoveries, and that the Planning Director shall determine the ultimate disposition of any discoveries.

Geology and Soils

- 16-1 For cut or fill slopes no taller than 30-feet and inclined at 2:1, proper slope maintenance procedures such as installation and maintenance of drainage devices and planting of slope faces to protect from erosion shall be implemented.
- 16-2 Surface water shall not be allowed to flow over the slopes other than incidental rainfall. No alteration of pad gradients should be allowed that will prevent pad and roof run-off from being expediently directed to approved disposal areas away from tops of slopes.
- 16-3 Top of slope berms shall be constructed and compacted as part of the precise grading plans shall be maintained by the property owner. The recommended drainage patterns shall be established at the time of finish grading and maintained throughout the life of the structures.
- 16-4 Concentrated surface waters entering the property from off-site sources shall be collected and directed to a permanent drainage system away from the top of slopes.
- 16-5 The applicant shall submit landscape plans to the Planning Department, and receive approval of prior of the plans prior to the issuance of a grading permit. The Landscape Plans shall include all of the natural area re-vegetation, parking lot landscaping, and wildland fire protection components.

Erosion

- 18-1 Prior to the issuance of a grading permit, the applicant shall submit a revegetation plan to the Planning Department for the proposed off-site haul route. The revegetation plan should include the use of appropriate native plant species and measures to prevent erosion.

Hazards and Hazardous Materials

- 20-1 All spills or leakage of petroleum products during construction activities shall be immediately contained; the hazardous material identified; and the contaminated site remediated in compliance with applicable state and local regulations regarding cleanup and disposal of the contaminant released. The contaminated waste shall be collected and disposed of at an appropriately licensed disposal or treatment facility.

Noise

- 32-1 Grading, earth removal, and construction shall be limited to daylight hours and during the hours of 7 a.m. to 6 p.m. on Monday through Friday, and 9 a.m. to 6 p.m. on

Saturday, and shall be prohibited on Sundays and federal holidays, except in emergencies.

- 32-2 Utilize construction methods or equipment that will provide the lowest level of noise impact, i.e., use newer equipment that will generate lower noise levels.
- 32-3 All construction vehicles and fixed or mobile equipment shall be equipped with properly operating and maintained mufflers or sound attenuation devices, as specified in regulations at the time of construction.
- 32-4 Schedule the construction such that the absolute minimum number of equipment would be operating at the same time.
- 32-5 All employees that will be exposed to noise levels greater than 75 dB over an 8-hour period shall be provided with adequate hearing protection devices to ensure no hearing damage will result from construction activities.
- 32-6 If equipment is being used that can cause hearing damage at adjacent noise receptor locations (distance attenuation shall be taken into account), portable noise barriers shall be installed that are demonstrated to be adequate to reduce noise levels at receptor locations below hearing damage thresholds. This may include erection of temporary berms or plywood barriers to create a break in the line-of-sight, or erection of a heavy fabric tent around the noise source.
- 32-7 The City shall require that a noise control plan be submitted for all impulsive construction equipment such as pile drivers, jackhammers, rockcrushers, etc, and all mobile equipment over 200 HP proposed to operate within 160 feet of an occupied residence as part of the grading permit application. The noise control plan shall outline measures to be taken to minimize noise nuisance and must be approved by the City prior to use of the impulsive construction equipment. Use of impulsive construction equipment such as pile drivers, jackhammers, rockcrushers, etc. will be limited during the regular school hours of the school buildings located within 1000 ft. of the noise generator.

Circulation

- 41-1 Prior to the initiation of any grading activities, the Planning Director and City Engineer shall determine if additional environmental analysis is required for the proposed haul component. No haul permit shall be issued with the prior approval of the Planning Director. The applicant shall obtain a haul route permit with a traffic control plan from the City Engineer. During construction activities, the traffic flow will be maintained to the highest level possible with the use of standard traffic control devices. Typical traffic control measures include warning signs, warning lights, and flaggers. Implementation of traffic control measures will provide guidance and navigational tools throughout the project area in order to maintain traffic flow and levels of safety during construction. At the City Engineer's sole discretion, additional mitigation measure or restrictions may be applied to the project to minimize impacts to the surrounding community. Additional environmental analysis will be required if the potential impacts exceed the impacts identified in this Initial Study.

VI. REFERENCES

- Engen Corporation, Geotechnical Feasibility Study Cornerstone Community C - Parking Lot and Ball Fields, March 8, 2006.
- Engen Corporation, Response to County Review Letter, BRGR060462, Dated May 16, 2006, June 28, 2006.
- Giroux & Associates, Air Quality Impact Analysis Cornerstones Church Expansion Riverside County, California, May 2008.
- Keller, Jean A. Ph.D., A Phase 1 Cultural Resources Assessment of Cornerstone Church Expansion, November 2004.
- Keller, Jean A. Ph.D., A Phase 1 Cultural Resources Assessment of Cornerstone Church Expansion, March 2009.
- Markham Development Management Group, Inc., Project Specific Preliminary Water Quality Management Plan, August 12, 2005.
- Musco, Green Generation Lighting, Lighting Survey, July 21, 2009
- Principe and Associates, MSHCP Compliance Report PUP 077R3, March 20, 2007.
- Scott White Biological Consulting, Wildomar Burrowing Owl Surveys, APN 367-140-008, 367-210-018, and 367-210-04, November 29, 2004.
- Scott White Biological Consulting, Wildomar Burrowing Owl Surveys, APN 367-140-008, 367-210-018, and 367-210-04, June 8, 2006.

FIGURES

Figure 2 – Vicinity Map



Figure 3 – Site Development Plan

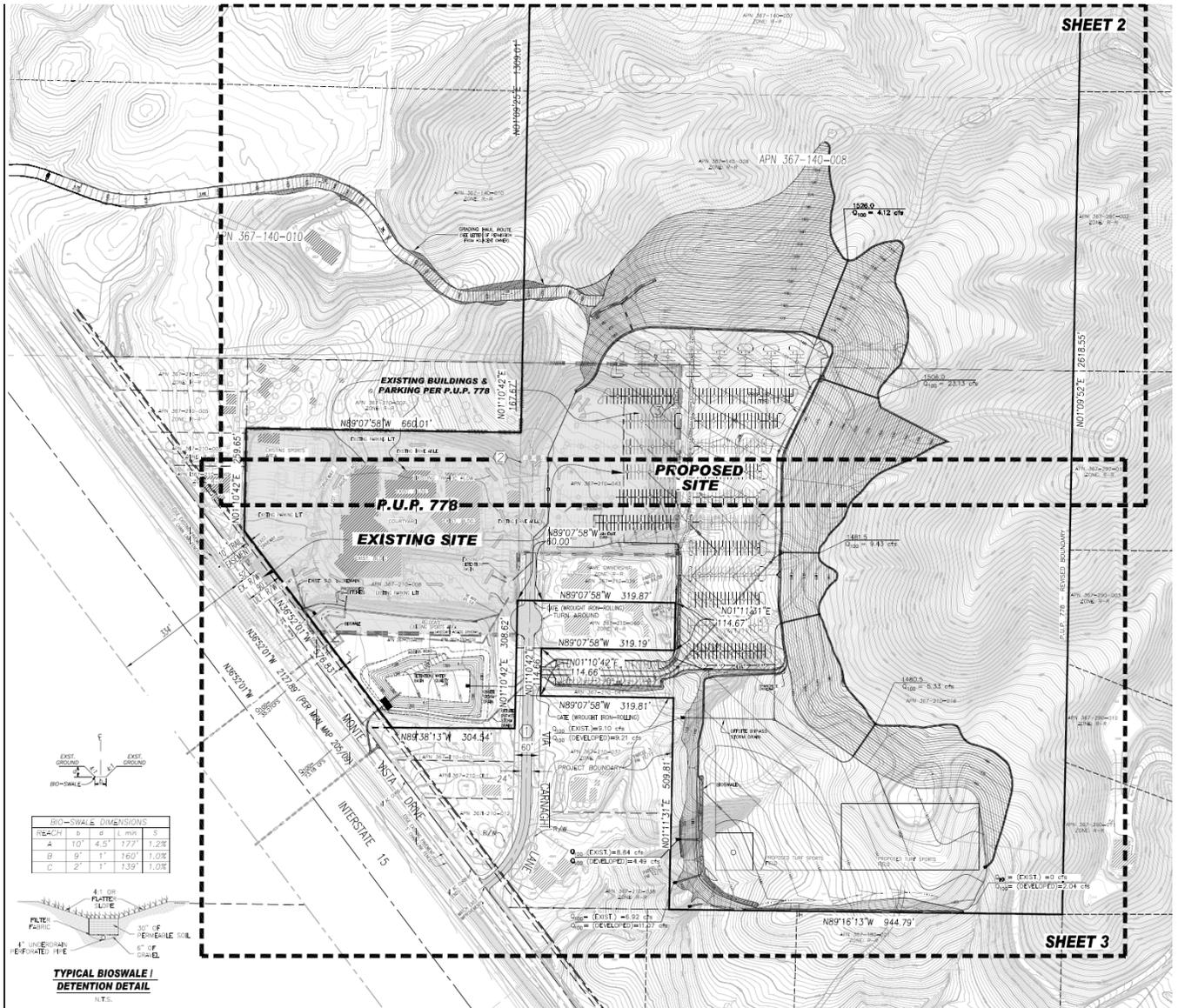


Figure 6 – Site Photographs



Figure 6.1 View of the Site from Interstate 15 looking east.

The proposed parking lot and athletic field improvements are located behind the trees and building.



Figure 6.2 View of the Site from Interstate 15 looking northeast.

The proposed athletic field improvements will be located in the middle portion of the picture.



Figure 6.3 Existing Upper Parking area located east of the existing church buildings.

The proposed haul route is proposed to occur over the low point in the hill near the middle of the picture.



Figure 6.4 Existing athletic field, future detention basin site south of the existing church buildings.



Figure 6.5 Location of proposed parking lot from the center of the site looking north.

The existing upper parking lot is located just to the left of this picture, the hills in the middle of the picture will be partially graded for the new lot expansion.

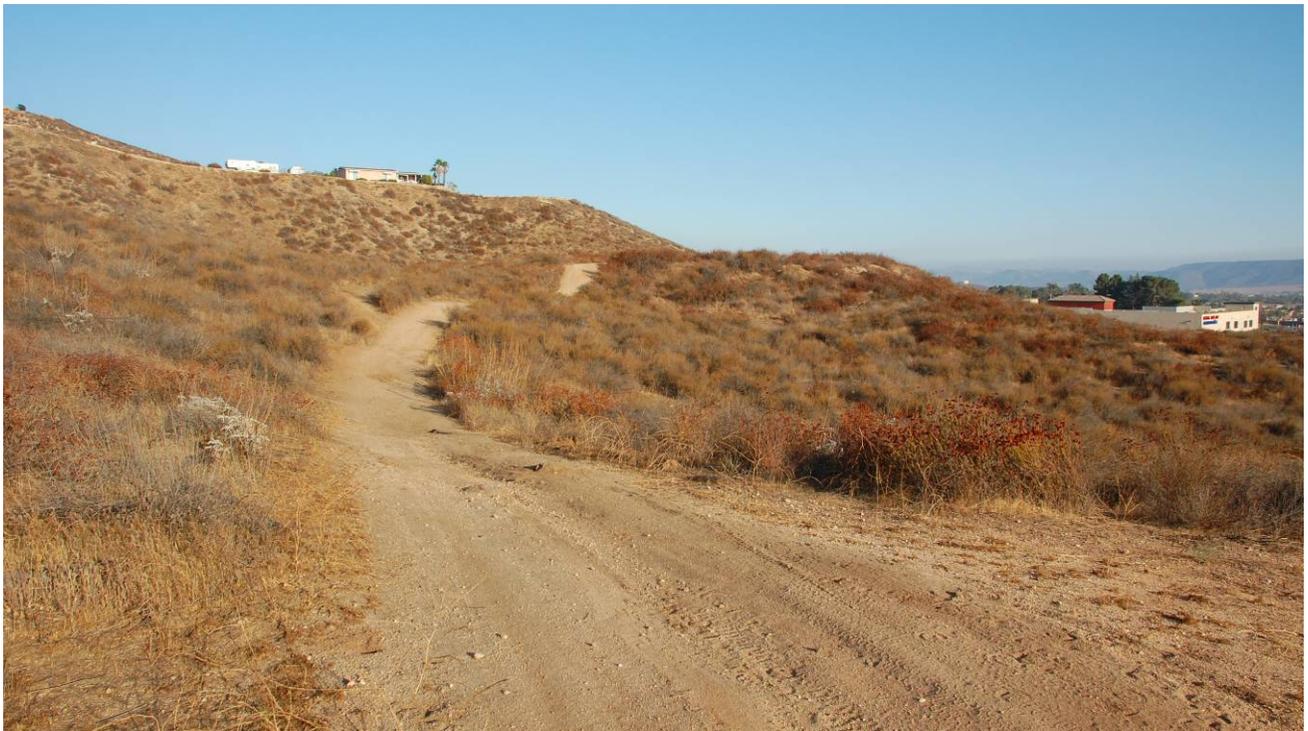


Figure 6.6 Location of proposed athletic fields from the center of the site looking southeast.

The existing residence which overlooks the proposed athletic fields is visible on the ridge top.



Figure 6.7 Location of proposed athletic fields looking west.



Figure 6.8 The main ridgeline on the east side of the proposed parking area and athletic fields.